

**ENVIRONMENTAL REVIEW COMMITTEE REPORT AND  
ADMINISTRATIVE SITE DEVELOPMENT PLAN REPORT & DECISION**

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| <b>ERC MEETING DATE:</b> | August 4, 2014  |
| <b>Project Name:</b>     | Cedar River Chevron   |
| <b>Owner:</b>            | Cedar River Station LLC, 15215 SE 272 <sup>nd</sup> St #201, Kent, WA 98042                 |
| <b>Applicant:</b>        | JP Athwal, Summit Petroleum Group, 20207 105 <sup>th</sup> Ave SE, Kent, WA 98031           |
| <b>Contact:</b>          | Roger Ollenburg, Roger Ollenburg Architect, 17405 33 <sup>rd</sup> Dr SE, Bothell, WA 98012 |
| <b>File Number:</b>      | LUA14-000799, ECF, MOD, SA-A  |
| <b>Project Manager:</b>  | Clark H. Close, Associate Planner   |

**Project Summary:** The applicant is requesting Administrative Site Plan Review and Environmental Review, for the construction of a new Chevron Fuel Station. The proposal includes a convenience store (3,612 SF), car wash (1,801 SF), and fueling facility under an overhead canopy (2,982 SF) with six double-sided pumps and two underground fuel storage tanks (20,000 gallon capacity), along with associated parking, landscaping and infrastructure. The development proposal is associated with the previously approved Cedar River Station project to the west and will use the existing storm water system. The 0.80 acre site is located within the Commercial Corridor (CC) land use designation and Commercial Arterial (CA) zoning classification. The subject property is located on the south side of Maple Valley Highway (SR 169). Primary access to the site is proposed off of Maple Valley Highway and secondary access is from 152<sup>nd</sup> Avenue SE. A total of eight (8) parking spaces are proposed. The site is located within Zone 1 and Zone 2 of the Wellfield Capture Zones and there are no other critical areas located onsite.

**Project Location:** 15355 Maple Valley Highway, State Route 169      **APN:** 232305-9211

**Site Area:** 35,031 SF (0.80 acres)      **Proposed New Bldg. Area (gross):** 8,395 SF

**STAFF RECOMMENDATION:** Staff Recommends that the Environmental Review Committee issue a Determination of Non-Significance Mitigated (DNS-M).



Project Location Map

**A. EXHIBITS:**

- Exhibit 1: Environmental Review Committee & Administrative Site Plan Report
- Exhibit 2: Cedar River Station Site Plan with Truck and Travel Path
- Exhibit 3: Site Plan
- Exhibit 4: Landscape Plan
- Exhibit 5: Building Elevations
- Exhibit 6: Canopy Elevations
- Exhibit 7: Original Geotechnical Report prepared by Earth Consulting Inc. (ECI), dated October 1, 2007
- Exhibit 8: Additional Geotechnical Report prepared by E<sup>3</sup>RA, dated November 22, 2013
- Exhibit 9: Site Plan Review Drainage Letter prepared by Beyler Consulting, dated October 10, 2013 and Cedar River Station Technical Information Report prepared by ESM Consulting Engineers, LLC, dated February 28, 2013
- Exhibit 10: Construction Mitigation Description
- Exhibit 11: Traffic Impact Analysis prepared by DN Traffic Consultants, dated August 23, 2012
- Exhibit 12: Preliminary Grading, Drainage, and Utility Plan
- Exhibit 13: Notice of Application and Proposed Determination of Non-Significance-Mitigated (DNS-M)
- Exhibit 14: Proof of Notice of Application Mailing
- Exhibit 15: Environmental "SEPA" Determination
- Exhibit 16: Wall Lighting Fixture
- Exhibit 17: Traffic Concurrency Test: Lee
- Exhibit 18: Aqua Barn Ranch, Inc. Development Agreement, recorded on March 7, 2007
- Exhibit 19: Aqua Barn Site Development Agreement Design Standards (Exhibit B of the Development Agreement)
- Exhibit 20: Cedar River Station Landscaping Plan, dated September 6, 2013
- Exhibit 21: Chevron Colors
- Exhibit 22: Advisory Notes to Applicant

**B. GENERAL INFORMATION:**

1. **Owner(s) of Record:** Cedar River Station LLC  
15215 SE 272nd St #201, Kent, WA 98042
2. **Comprehensive Plan Land Use Designation:** Commercial Corridor (CC)
3. **Zoning Designation:** Commercial Arterial (CA)
4. **Existing Site Use:** Vacant
5. **Neighborhood Characteristics:**
  - a. **North:** *Maple Valley Highway (SR 169); Cedar River Trail, King County Parks - Vacant - Single Family (RA5 zone)*
  - b. **East:** *Residential Mobile Home Park (RMH zone) - Condominium, Emerald Crest*
  - c. **South:** *Multi-Family Residential (R-14 zone) – Valley Springs Apartments*
  - d. **West:** *Church/Welfare/Religious Services (R-14 zone) – New Life Church @ Renton*

6. Site Area: 35,031 SF

### C. HISTORICAL/BACKGROUND:

| <u>Action</u>                               | <u>Land Use File No.</u> | <u>Ordinance No.</u> | <u>Date</u> |
|---|--------------------------|----------------------|-------------|
| Comprehensive Plan                          | N/A                      | 5099                 | 11/01/2004  |
| Zoning                                      | N/A                      | 5100                 | 11/01/2004  |
| Annexation                                  | N/A                      | 5373                 | 06/09/2008  |
| Master Plan, Cedar River Station            | LUA12-000193             | N/A                  | 05/02/2013  |
| Aqua Barn Ranch, Inc. Development Agreement | N/A                      | N/A                  | 03/07/2007  |

### D. PUBLIC SERVICES:

#### 1. Existing Utilities

- a. Water: Water service will be provided by Cedar River Water District. Water utilities will be designed by the Water District.
- b. Sewer: Sewer service will be provided by Cedar River Sewer District. Sewer utilities will be designed by the Sewer District.
- c. Surface/Storm Water: All exposed impervious areas will be collected in catch basins and conveyed in a piped conveyance system to an underground water quality and detention vault. Discharge from the vault will be into the Lower Cedar River King County Drainage Basin.

2. **Streets**: Maple Valley Highway (SR 169) and 152<sup>nd</sup> Avenue SE

3. **Fire Protection**: City of Renton Fire Department

### E. APPLICABLE SECTIONS OF THE RENTON MUNICIPAL CODE:

#### 1. Chapter 2 Zoning Districts – Uses and Standards

- a. Section 4-2-020: Purpose and Intent of Zoning Districts
- b. Section 4-2-060: Zoning Use Table – Uses Allowed in Zoning Designations
- c. Section 4-2-120: Commercial Development Standards

#### 2. Chapter 3 Environmental Regulations and Overlay Districts

- a. Section 4-3-050: Critical Area Regulations
- b. Section 4-3-100: Urban Design Regulations

#### 3. Chapter 4 City-Wide Property Development Standards

#### 4. Chapter 6 Street and Utility Standards

- a. Section 4-6-060: Street Standards

#### 5. Chapter 9 Permits – Specific

- a. Section 4-9-200: Master Plan and Site Plan Review

## 6. Chapter 11 Definitions

### F. APPLICABLE SECTIONS OF THE COMPREHENSIVE PLAN:

1. Land Use Element (2007)
2. Community Design Element (2007)

### G. ENVIRONMENTAL REVIEW

In compliance with RCW 43.21C.240, the following environmental (SEPA) review addresses only those project impacts that are not adequately addressed under existing development standards and environmental regulations.

#### 1. Environmental Threshold Determination

- a. Based on analysis of probable impacts from the proposal, staff recommends that the Responsible Officials:

**Issue a DNS-M with a 14-day Appeal Period.**

#### 2. Mitigation Measures

- a. All earthwork performed or implemented by the applicant shall be consistent with the recommendations of the Geotechnical Engineering Study prepared by Earth Consulting Incorporated (ECI), dated October 1, 2007 (*Exhibit 7*).
- b. If any Native American grave(s) or archaeological/cultural resources (Indian artifacts) are found, all construction activity shall stop and the owner/developer shall immediately notify the City of Renton Planning Division, concerned Tribes' cultural committees, and the Washington State Department of Archaeological and Historic Preservation.

#### 3. Environmental Impacts

*The Proposal was circulated and reviewed by various City Departments and Divisions to determine whether the applicant has adequately identified and addressed environmental impacts anticipated to occur in conjunction with the proposed development. Staff reviewers have identified that the proposal is likely to have the following probable impacts:*

##### a. Earth

**Impacts:** The ground surface in the vicinity of the subject site is relatively flat, graded and ready for construction. The site's steepest slope is approximately 2 percent. The ground surfaces around the perimeter of the fill and graded site slopes towards the north, south and east property lines. Following development, impervious surfaces will be collected in catch basins and conveyed in a piped conveyance system to an underground water quality and detention vault located on the companion property to the west (*Exhibit 2*); impervious surfaces would be at 78 percent.

According to the Geotechnical Engineering Report prepared by the E<sup>3</sup>RA, dated November 22, 2013, the soils encountered during field exploration in the top 7.5 feet include dark brown, moist, medium dense silty sand with some gravel (SM) that transitions to brown with trace gravel after 3 feet. From 7.5 feet to 9 feet the soils consisted of brown, slightly moist, medium dense, fine to medium sand (SP) (*Exhibit 8*). No groundwater or test pit caving was encountered in either test pit conducted by E<sup>3</sup>RA. The original Geotechnical Report prepared by ECI, dated October 1, 2007, excavated two test pits in close proximity to the fuel station and they encountered silty sand or sand with silt in the upper 5.5 to 7.5 feet, underlain by silty or poorly graded gravel (*Exhibit 7*). The results of both subsurface investigations conducted are

generally consistent, which was expected since the entire building pad appears to have been constructed at the same time.

The applicant proposes to excavate approximately 1,091 cubic yards and backfill 912 cubic yards (726 cu. yds. of pea gravel, 152 cu. yds. of crushed rock, and 34 cu. yds. of gravel). The applicant will be required to maintain the existing Temporary Erosion and Sedimentation Control Plan (TESCP) pursuant to the 2009 King County Surface Water Design Manual Erosion and Sediment Control Requirements.

The submitted geotechnical report provides recommendations for site preparation and earthwork, foundations, retaining walls, slab-on-grade floors, seismic design considerations, excavations and slopes, site drainage, utility support and backfill, and suggested pavement sections. Grading will involve cuts and fills of less than three (3) feet on the relatively level property. The report concludes that the development of the project be completed in accordance with the recommendations put forth in the 2007 geotechnical report for Cedar River Station. Staff recommends the project proponent follow the recommendations presented in the geotechnical study regarding site preparation and general earthwork, Structural Fill, Foundations, Retaining Walls, Slab-on-Grade Floors, Seismic Design Considerations, Excavations and Slopes, Site Drainage, Utility Support and Backfill, and Suggested Pavement Sections.

**Mitigation Measures:** Project construction shall be required to comply with the recommendations outlined in the Geotechnical Engineering Study prepared by Earth Consulting Inc., dated October 1, 2007 (*Exhibit 7*).

**Nexus:** SEPA Environmental Review, RMC 4-4-060 Grading, Excavation and Mining Regulations.

#### **b. Environmental Health Hazards**

**Impacts:** It is not anticipated that the project would significantly impact environmental health. The applicant is proposing two underground fuel storage tanks, 20,000 gallons total capacity, located west of the fueling station islands. The underground tanks measure roughly 30 feet by 42 feet (1,260 SF) and are at least 120 feet from the residential neighbors to the south and east. The following measures will be used to reduce or control surface, ground, and runoff impacts: underground tanks fill connection will be surrounded by containment manholes allowing any fuel trapped in the container during the tank fill process to discharge into the tank; underground tanks fill and vent systems are designed with a prevention system to stop the flow of product into the tanks at a predetermined level below the top of the tank; underground tanks have an inert outer wall and are designed with a double wall system should the inner tank leak or fail; tank mounted pumps are installed within sumps attached to the tanks; the product pipes use a double wall system that slopes downward from the dispensers to the tank sumps; inert sumps exist under all dispensers; electronic sensors are installed to monitor the tank interstitial spaces, tanks sumps and dispenser sumps.

**Mitigation Measures:** No further mitigation recommended.

**Nexus:** Not applicable.

#### **c. Air**

**Impacts:** It is not anticipated that the project would significantly impact air quality. Any impacts would be from construction and future operations, including customer traffic. Air impacts from construction would be temporary and would be limited in duration. The Temporary Erosion and Sediment Control Plan (TESCP) would aid in reducing the airborne particulates related to dust by controlled watering of the grounds during construction.

Following construction, the operation of the fuel center would result in such emissions as carbon monoxide (CO), nitrogen oxides (NOXs), sulfur oxides (SOXs), volatile organic compounds (VOCs), hydrocarbons, and

related airborne pollutants typically associated with the use of fossil fuels. Gas fumes must meet federal air quality standards. The project would need to meet the Puget Sound Clean Air Agency requirements to minimize impacts of idling cars and dispensers. These systems would reduce and/or recover vapors.

**Mitigation Measures:** No further mitigation recommended.

**Nexus:** Not applicable.

#### d. Storm Water

**Impacts:** A Preliminary Technical Information Report (TIR) prepared by ESM Consulting Engineers, LLC, dated February 28, 2013, and Site Plan Review Drainage Letter prepared by Beyler Consulting, dated October 10, 2013, was submitted with the application (*Exhibit 9*). The provided TIR identifies that the site is nearly flat without much slope; stormwater runoff will be collected in catch basins and conveyed in a piped conveyance system to the existing stormwater water quality and detention vault (located on Lot A). The new pipes will connect to the storm system of the Cedar River project (designed to manage stormwater for the Cedar River Station on Lot A and Cedar River Chevron on Lot B). The vault detains stormwater to required flow control standards before releasing runoff downstream. The storm system will comply with City of Renton Amendments to the 2009 King County Surface Water Design Manual (KCSWDM) for the flow control and water quality facility analysis and design. All commercial projects require enhanced basic water quality treatment. The developed site runoff will be released into an existing catch basin along SE Maple Valley Road (SR 169) and travel along a pipe/ditch conveyance system until runoff enters Cedar River a couple miles downstream.

The proposed project would result in more than 5,000 SF of new impervious surface area; therefore, an onsite stormwater flow control system is required. Level 2 conservation flow control is proposed to be used. A project specific temporary erosion and sediment control (TESC) plan will be prepared during final engineering.

**Mitigation Measures:** No further mitigation recommended.

**Nexus:** Not applicable.

#### e. Noise

**Impacts:** During construction, equipment operation would temporarily increase noise levels in the vicinity of the project. Construction vehicles are required to have mufflers to reduce some of the temporary impact of construction noise. In addition, the applicant has indicated they would limit construction to the standard hours permitted by the City of Renton (*Exhibit 10*).

The project would result in long-term noise increases due to the vehicular activities associated with a commercial development including a drive-through and delivery vehicles. The proposed site plan placed trash receptacles and delivery areas at least 50 feet north of the south property line where the site abuts residential development. In addition, a 5-foot high wood fence and a landscaped buffer will provide an environmental barrier from the residential development to the south and east. Moreover, the increase in noise is anticipated to be minimal compared to the traffic noise associated with Maple Valley Highway located immediately north of the site.

**Mitigation Measures:** No additional mitigation needed.

**Nexus:** Not applicable.

**f. Historic and Cultural Preservation**

**Impacts:** Historically the Cedar River has meandered downstream in the Renton-Maple Valley area across the width of the river valley. Furthermore, developments within the vicinity of the Cedar River are more likely to be sites where significant historic and/or cultural resources would be found. The subject site was conditioned under LUA12-000193 to stop work and immediately notify the City of Renton Planning Division, concerned Tribes' cultural committees, and the Washington State Department of Archeological and Historic Preservation if any Native American grave(s) or archaeological/cultural resources (Indian artifacts) are found, as such staff recommends the same mitigation measure for the subject project.

**Mitigation Measures:** If any Native American grave(s) or archaeological/cultural resources (Indian artifacts) are found, all construction activity shall stop and the owner/developer shall immediately notify the City of Renton Planning Division, concerned Tribes' cultural committees, and the Washington State Department of Archeological and Historic Preservation.

**Nexus:** SEPA Environmental Regulations.

**g. Transportation**

**Impacts:** Access to the site is proposed to be at two locations, one from Maple Valley Highway (SR 169) and one from 152nd Ave SE. The access from SR 169 is right-in and right-out only and the access from 152<sup>nd</sup> Ave SE will be a full access with both left and right turning movements. Right-of-way improvements are proposed along Maple Valley Highway and currently exist along 152nd Ave SE. In addition, eight (8) parking stalls are proposed in a surface parking lot to serve the proposed development.

The applicant submitted a Traffic Impact Analysis prepared by DN Traffic Consultants, dated August 23, 2012 (*Exhibit 11*). The provided report includes analysis of the existing transportation system, containing public transit and pedestrian facilities for the entire Cedar River Station and the proposed fuel station. Based on the provided study, the site is served by Metro Transit Route 143 and Route 907. Both routes provide transit from Enumclaw to Renton for weekdays; however, no weekend service is provided or proposed for the Cedar River Station. The nearest bus stops are located on the northwest corner of SR 169/152nd Ave SE intersection for westbound service and southeast corner for eastbound service. There are existing pedestrian improvements (sidewalks) along 152nd Ave SE and along SR 169 west of 152nd along the front of the New Life Church property. Across SR 169 is the Cedar River Trail, a multi-purpose recreational trail that extends from Lake Washington to Maple Valley.

The Traffic Analysis assumed a horizon year of 2014 for build out of the Cedar River Station, and analyzed traffic impacts at the following intersections and access points:

- SR 169/140th Way SE
- SR 169/149th Ave SE
- SR 169/152nd Ave SE/154th Pl SE
- SR 169/Molasses Condo Access
- SR 169/145th Ave SE
- SR 169/161st Ave SE
- SR 169/Site Access
- 152nd Ave SE/Site Access

The development proposal is associated with the previously approved Cedar River Station project to the west. The calculated average weekday daily trips, for Cedar River Chevron was estimated at 1,953 using the average weekday daily trip generation rate. Cedar River Station, as a single use, is estimated to generate 5,898 daily, 486 AM peak and 359 PM peak hour trips. The entire Cedar River Station, including the fuel station, is estimated to generate 105 PM peak trips (47 of which are attributed to Cedar River Chevron) on the adjacent arterial network with 133 trips to internal capture and 121 to pass by trips (359 - 133 - 121 = 105 total PM peak hour trips). The project passes the City of Renton Traffic Concurrency Test per RMC 4-6-070.D (*Exhibit 17*).

The traffic study included a level of service analysis completed for the above listed intersections/access points. This qualitative level of service remains the same for both existing 2012 PM peak hour and future 2014 PM peak hour. The conclusion of the evaluation is that the Cedar River Station, including Cedar River Chevron, would not create a significant adverse impact on traffic operations at the analysis intersections.

A crash history analysis was also completed by DN Traffic Consultants. The report identifies that there has been a total of 53 crashes at three of the six study area intersections on SR 169 during the last three plus years. However, the analysis concludes that there is not a significant frequently occurring crash event that would warrant consideration for mitigation, as the overall crash history is significantly less than the statewide average for signalized intersections.

The provided analysis concluded that traffic generated by the proposed Cedar River Station is not expected to create a significant adverse impact on the City of Renton roadway network.

However, it should be noted that a Development Agreement between the City and the property exists and is recorded under recording number 20070307000134 (*Exhibits 18 & 19*). Among other things, the Development Agreement includes a traffic mitigation fee credit. As identified in Section 4 of the Development Agreement, in 2003 the signalized intersection and turn lanes and other improvements were constructed with private funds totaling \$337,066. It was identified that the commercial site (subject site) would contribute an estimated 75 percent share of the traffic trips to the intersection. Based on this analysis, the City agreed that a sum of \$252,799.50 would be credited against the Renton traffic mitigation fees (now impact fees) due upon development of the property.

**Mitigation Measures:** No further mitigation recommended.

**Nexus:** Not applicable.

#### **h. Fire & Police**

**Impacts:** Police and Fire Prevention staff indicated that sufficient resources exist to furnish services to the proposed development, subject to the condition that the applicant provides Code required improvements and fees.

**Mitigation Measures:** No further mitigation recommended.

**Nexus:** Not applicable.

#### **i. Comments of Reviewing Departments**

The proposal has been circulated to City Department and Division Reviewers. Where applicable, their comments have been incorporated into the text of this report and/or "Advisory Notes to Applicant" (*Exhibit 22*).

- ✓ **Copies of all Review Comments are contained in the Official File and may be attached to this report.**

### **H. ADMINISTRATIVE SITE PLAN REVIEW FINDINGS OF FACT:**

1. The applicant, JP Athwal, Summit Petroleum Group, is requesting Administrative Site Plan Review and Environmental "SEPA" Review for the development of Chevron Fueling Station that includes a 3,612 square foot convenience store, a 2,982 square foot canopy over six fueling drive lanes each with two fueling stations, a 1,801 square foot car wash with a mechanical room, two underground fuel storage tanks with a total capacity of 20,000 gallons, and associated improvements.

2. The 0.80 acre site, or Lot B (APN 232305-9211), is located within the Commercial Corridor (CC) land use designation and Commercial Arterial (CA) zoning classification.
3. The proposed building heights are as follows: convenience store 19'-8", fuel canopy 20'-0", and car wash 18'-0".
4. The development of the subject site includes eight (8) surface parking stalls and associated utility improvements, such as water service, sanitary sewer, and gas connections. Associated drainage improvements include catch basins conveyed in a piped conveyance system to the existing stormwater water quality and detention vault located on parcel no. 232305-9210 (Lot A) as part of the approved system for Cedar River Station, Master Site Plan (LUA12-000193).
5. Cedar River Station Master Site Plan and Site Plan review was approved by the Renton Hearing Examiner on March 8, 2013. Development of the 21,703 SF parcel consists of general retail space in three separate buildings, 97 parking stalls and two loading stalls.
6. The Master Site Plan identified a gas station on the eastern edge of the proposal. The gas station was not included in the subject site plan review and/or SEPA Environmental Review, however it was included to be a "place holder" and facilitate a Master Site Plan for the combined site.
7. The development is subject to a Development Agreement Recording #20070307000134, which includes but is not limited to, site specific land use restrictions, traffic mitigation credits, and development regulation vesting.
8. The applicant is requesting a stacking modification in order to allow stacking for up to 12 vehicles instead of the required 30 vehicles. The project fueling area will have six fueling drive lanes each with two fueling locations.
9. The subject property is located on the south side of Maple Valley Highway (SR 169), just east of 152<sup>nd</sup> Ave SE.
10. The applicant is proposing two (2) access locations, one from Maple Valley Highway (SR 169) and one from 152nd Ave SE. The access from SR 169 is right-in and right-out only and the access from 152nd Ave SE is a full access with both left and right turning movements.
11. The site is bordered to the south by Valley Springs Apartments, to the west by New Life Church @ Renton, to the east by Emerald Crest Condominiums, and to the north by SR 169.
12. The massing of the proposed structures are most prominently visible from SR 169.
13. The proposed structures have varied roof elements, including extended parapets with metal flashing and metal siding.
14. Building materials include smooth face CMU, split face CMU, steel columns, steel framing, metal panels, pre-finished metal siding, metal awnings, ACM panels, and DRYVIT siding (*Exhibits 5 & 6*).
15. Exterior building colors include nutmeg, café au lait, chilled wine, mocha, and noir (*Exhibits 5 & 21*).
16. Canopy fascia, canopy columns include blue, silver, white, sierra tan and dark gray (*Exhibits 6 & 21*).
17. The entrance to the convenience store ("Extra Mile") is along the northern façade with an orientation toward Maple Valley Highway. The fuel station is located north of the convenience store, and the car wash is located east of the convenience store and the fuel station.
18. The site is located within Zone 1 and Zone 2 of the Wellfield Capture Zones and there are no other critical areas located on site.
19. The ground surface in the vicinity of the subject site is relatively flat. The site steepest slope is approximately 2 percent.

20. No trees are located on the site.
21. The applicant proposes to excavate approximately 1,091 cubic yards and backfill 912 cubic yards (726 cu. yds. of pea gravel, 152 cu. yds. of crushed rock, and 34 cu. yds. of gravel).
22. The conceptual landscape plan submitted with the application includes the installation of landscaping both along the perimeters of the site and internal to the site. Vegetation proposed includes a variety of trees: Jacquemontii Birch, Katsura, Crimson Pointe Plum, Weeping Alaska Cedar, Hinoki Cypress, Vanderwolf's Pyramid Limber Pine; shrubs: Compact Strawberry Bush, Everest Azalea, Cherry Bomb Barberry, Rose Glow Japanese Barberry, Mexican Orange, White Rock Rose, Velvet Cloak Smoke Tree, Flaming Silver Japanese Pieris, Goldmound Spirea, Miss Kim Lilac; and groundcover: Kinnikinnick, Pink Panda Sand Strawberry, and seasonal color.
23. The Planning Division of the City of Renton accepted the above master application for review on June 20, 2014 and determined it complete on July 3, 2014 (*Exhibits 13 & 14*). The project complies with the 120-day review period.
24. A drainage report has been submitted by ESM Consulting Engineering, along with a site plan review drainage letter prepared by Beyler Consulting (*Exhibit 9*). A Level 1 analysis was done and the report addresses compliance with 2009 King County Surface Water Manual and City of Renton Amendments to the KCSWM, Chapters 1 and 2. All core and special requirements have been discussed in the report. Based on the City's flow control map, this site falls within the Flow Control Duration Standard, Forested Conditions. The engineer has provided calculations, and noted in the report a combined detention and water quality vault will be constructed off-site under the parking lot of Lot A.
25. A geotechnical report was submitted by Earth Consulting, Inc (ECI). The report identifies the soils as glacial till and notes groundwater is present 7-10 feet below the surface. These soils will not support infiltration. Roof downspouts will be tightlined to the storm system. A site-specific geotechnical report was submitted by E<sup>3</sup>RA (*Exhibit 8*). No groundwater or test pit caving was encountered in either test pit conducted by E<sup>3</sup>RA. The results of both subsurface investigations conducted are generally consistent with geotechnical report prepared by ECI.
26. No public comments were received.
27. Pursuant to the City of Renton's Environmental Ordinance and SEPA (RCW 43.21C, 1971 as amended), on August 4, 2014, the City's Environmental Review Committee issued a Determination of Non-Significance-Mitigated (DNS-M) (*Exhibit 15*). A 14-day appeal period will commence with this Administrative Site Plan Review Decision on August 8, 2014 and end on August 22, 2014.
28. Representatives from various city departments have reviewed the application materials to identify and address issues raised by the proposed development. These comments are contained in the official file, and the essence of the comments have been incorporated into the appropriate sections of this report and the Departmental Recommendation at the end of this report.
29. The proposal requires Site Plan Review. The following table contains project elements intended to comply with Site Plan Review decision criteria, as outlined in RMC 4-9-200E:

**SITE PLAN REVIEW CRITERIA:**

**a. Comprehensive Plan Compliance and Consistency:**

The site is designated Commercial Corridor (CC) on the Comprehensive Plan Land Use Map. The purpose of CC is to evolve from "strip commercial" linear business districts to business areas characterized by enhanced site planning incorporating efficient parking lot design, coordinated access, amenities, and boulevard treatment. **The proposal is compliant with the vested 2007 Development Agreement and the following Comprehensive Plan policies if all conditions of approval are met.**

|   |   |
|---|---|
| ✓ | <b>Objective LU-EEE:</b> Create opportunities for development and re-development of land in portions of the Commercial Corridor designation for general business and service uses.  |
| ✓ | <b>Policy LU-346.</b> Support the redevelopment of commercial business districts located along principal arterials in the City.   |
| ✓ | <b>Policy LU-347.</b> Implement development standards that encourage lively, attractive, medium to high-density commercial areas.   |
| ✓ | <b>Policy LU-348.</b> Encourage consolidation of individual parcels to maximize flexibility of site design and reduce access points.  |
| ✓ | <b>Policy LU-349.</b> Support development plans incorporating the following features:<br>1) Shared access points and fewer curb cuts;<br>2) Internal circulation among adjacent parcels;<br>3) Shared parking facilities;<br>4) Allowance for future transition to structured parking facilities;<br>5) Centralized signage;<br>6) Unified development concepts; and<br>7) Landscaping and streetscape that softens visual impacts. |
| ✓ | <b>Policy LU-353.</b> Structures at Commercial Corridor intersections should not be set back from the street and sidewalk so as to allow vehicular circulation or parking to be located between the sidewalk and the building.  |
| ✓ | <b>Policy LU-358.</b> Parking areas should be landscaped (including street trees, buffers, berms), especially along roadways, to reduce visual impacts.   |
| ✓ | <b>Objective LU-JJJ:</b> Where Commercial Corridor areas intersect other land use designations, recognition of a transition and/or buffer between uses should be incorporated into redevelopment plans.   |
| ✓ | <b>Policy LU-368.</b> Consideration of the scale and building style of nearby residential neighborhoods should be included in development proposals.  |
| ✓ | <b>Policy LU-369.</b> Development should be designed to consider potential adverse impacts on adjacent, less intensive uses, e.g. points of ingress and egress, lighting, landscaping, pedestrian walkways, and setbacks should all be considered during site design.   |
| ✓ | <b>Policy LU-370.</b> Landscape buffers, additional setbacks, reduced height, and screening devices such as berms and fencing should be employed to reduce impacts (e.g. visual, noise, odor, light) on adjacent, less intensive uses.  |

**b. Zoning Compliance and Consistency:**

Pursuant to Section 5. *Effect of Development Agreement*, the property shall not be subject to a new zoning ordinance or an amendment to a zoning ordinance or to a development regulation or standard adopted by the City after the effective date of the Agreement, unless agreed to by the owners of the portions of the property. The City and the Owners have agreed the attached development regulations would be applicable to the subject development. Such vested standards are reflected below.

The subject site is classified **Commercial Arterial (CA)** on the City of Renton Zoning Map. The following development standards are applicable to the proposal:

**Density:** Per RMC 4-2-120A the allowed density range in the CA zoning classification is a minimum of 20 dwelling units per net acre (du/ac) up to a maximum of 60 du/ac. Net density is calculated after public rights-of-way, private access easements, and critical areas are deducted from the gross acreage of the site.

Not applicable.

**Lot Dimensions:** Per RMC 4-2-120A the minimum lot size, in the CA zone, is 5,000 square feet.

Not applicable.

**Maximum Lot Coverage for Buildings:** Per RMC 4-2-120A 65% of total lot area or 75% if parking is provided within the building or within an on-site parking garage.

Three commercial buildings would be located on Lot B, which are proposed to have a combined building footprint of 8,395 SF, which would result in 24% building coverage. As such, the project would be in compliance with the maximum building coverage standards.

**Setbacks:** Per RMC 4-2-120A the CA zoning classification requires a minimum front yard setback of 10 feet which may be reduced to zero feet during the site plan development review process, provided blank walls are not located within the reduced setback. The CA zone has no rear or side yard setback other than 15 feet if lot abuts or is adjacent to a residential zone, not including RMH. The minimum freeway frontage setback of 10 feet landscaped from the property line.

The applicant provided a site plan with the application (*Exhibit 3*); based on the site plan, all buildings would exceed the minimum setbacks. The closest building to the property line is the Car Wash/Mechanical building, located 15 feet from the east property line. The applicant has proposed a 15'-6" landscape bed along the south and north property lines, and a 15-foot or greater landscaped setback on the east side. No landscaping is proposed between the property lines of Cedar River Station retail development to the west and Cedar River Chevron.

**Building Height:** Per RMC 4-2-120A building height is restricted to 50 feet unless a conditional use permit is obtained.

The height of any of the proposed structures would roughly be 20 feet or less (*Exhibits 5 & 6*), which is compliant with the maximum height permitted in the CA zone.

#### **Landscaping:**

##### Site Landscaping:

Landscaping requirements are identified in *Exhibit B* of the Development Agreement, subsection Standards 3, which requires a 15-foot wide landscape strip along all street frontages (*Exhibit 19*). Unimproved portions of abutting street right-of-way can be used in combination with abutting private property to meet the required 15-foot landscape strip width. Such landscaping shall include a minimum 30-inch high berm and 2.5 inch caliper red maples planted 25 feet on center, if the existing trees are not retained.

In addition to the street frontage landscaping requirements, RMC 4-2-120A requires a 15-foot wide landscape visual barrier or (if approved via site plan review) a 10-foot wide sight-obscuring landscape strip between commercial lots and abutting property zoned residential (excluding RMH). In addition, the Development Agreement requires a double-faced and stained 5-foot high wood fence installed along any unfenced boundaries of the site that abut residentially zoned property. In order to improve pedestrian access to and from abutting residential properties, openings in the fence shall be provided.

The applicant provided a conceptual landscape plan with the application (*Exhibit 4*), which identifies a

minimum 15-foot wide landscape strip along all frontages with a berm located in the planter strip on SR 169, and a 5-foot high wood fence located along the south and east properties for additional screening. All existing street trees along SR 169 (Maple Valley Highway) have been removed. As such, the applicant was conditioned to replace the street trees on SR 169 with 2.5 inch caliper red maples. Cedar River Station Landscaping Plan, as approved on September 9, 2013 (*Exhibits 18 & 19*), included three new 2.5 inch caliper Quaking Aspen trees with 40-foot spacing as replacement trees identified in Exhibit B, section 3, of the Development Agreement in the WSDOT right-of-way along SR 169. The approved landscaping plan for Cedar River Station satisfies the provisions of the Development Agreement. The applicant's conceptual landscape plan is generally consistent with the Cedar River Station's landscaping plan. The applicant shall provide final landscape plan for review and approval prior to construction permit issuance.

The applicant has proposed to provide a 15.5-foot wide landscape strip along a portion of the rear property line adjacent to the R-14 zoned property. The landscaping transitions down to an 8-foot wide landscape strip at the southwest corner of the lot, in order to match the approved site plan for Cedar River Station to the west.

Staff recommends as a condition of approval that additional landscaping trees be placed on the east elevation in order to better screen the mechanical building and car wash building from the residential development to the east. The applicant shall provide a revised landscaping plan for review and approval by the Development of Community & Economic Development, Planning Division project manager.

Parking Lot Landscaping:

*Surface parking lots with less than fourteen (14) stalls are not required to provide minimum amounts of interior parking lot landscaping. At Cedar River Station (Lot A), 97 parking stalls were identified. Therefore, a minimum of 25 SF of parking lot landscaping per stall was required (15 SF when total number of parking stalls is between 15 and 50). Additional code requirements were as follows: any landscaped area shall be a minimum of 5 feet in width. Interior parking lot landscaping shall include at least one tree for every 6 spaces; shrubs shall be planted at a rate of 5 per 100 SF of landscaped area. Up to 50% of the shrubs may be deciduous and all shrubs shall have a mature height between three and four feet. Groundcover shall be sufficient quantities to provide 90% coverage within three years of installation. No more than 50 feet shall be permitted between parking stall landscape areas. An underground Irrigation System is required.*

Based on the proposal to provide 8 parking stalls, the applicant should provide interior parking lot landscaping to match the thresholds provided by Cedar River Station, given both parcels were approved under the same master site plan review. The applicant is proposing two planter islands totaling roughly 175 SF with 2 trees, 10 shrubs, and additional groundcover. The conceptual landscape plan provides roughly 21½ SF of interior parking lot landscaping per stall. All parking stalls are within 50 feet of a landscaped area. An additional 150 SF of interior landscaping is provided on the north and south side of the refuse and recycling facility. The proposed parking lot landscaping plant schedule, locations, and areas comply with the standards of the code as submitted by the applicant. The applicant shall provide an underground irrigation system with the building permit application for review and approval by the Development of Community & Economic Development, Planning Division project manager.

***Refuse and Recyclables:*** Per RMC 4-4-090 for retail developments a minimum of 5 square feet per every 1,000 square feet of building gross floor area shall be provided for recyclable deposit areas and a minimum of 10 square feet per 1,000 square feet of building gross floor area shall be provided for refuse deposit areas with a total minimum area of 100 square feet. In addition, such facilities are required to be screened and gated pursuant to the above code section.

*Outdoor refuse and recyclables deposit areas and collection points shall not be located within fifty feet of a residentially zoned property.*

*Garbage dumpsters, refuse compactor areas, and recycling collection areas must be fenced or screened. A six foot (6') wall or fence shall enclose any outdoor refuse or recyclables deposit area.*

Based on the proposal for a total of 8,395 SF retail establishment, a minimum area of 42 SF of recycle area and 84 SF of refuse area would be required. The applicant is proposing a 300 square foot refuse and recyclable deposit area near the retail store. The proposal exceeds the refuse and recyclable size standards. The facility's southern wall is located approximately 58 feet north of the south property line, complying with the minimum distance requirements from the adjacent residential development. Based on the provided trash enclosure details, the facility would be enclosed with a CMU wall, metal roof, and a gate. The CMU wall stands 7.5 feet tall and is covered by a metal roof. The parking lot opening measures 18.5-foot wide with two 10-foot long gate leaves that are offset by 3.5 feet. A second pedestrian gate opens toward the west wall of the convenience store. The refuse and recycling facility complies with the code regulations.

The proposed facility is more than twice the required size required by code and therefore should be reduced in size to enhance the visual quality of the proposed development, and also provide consistency with the master plan approved for Cedar River Station. As a result, staff recommends as a condition of approval that the refuse and recycling facility be reduced in sized to no greater than 144 SF in area and no less than 126 SF (and shall maintain a gate opening at least twelve feet (12') wide for haulers per RMC 4-4-090C.9). The colors should also match the convenience store colors by providing two (2) courses of CMU in mocha color followed by nine (9) courses of CMU in café au lait color. The roof portion should be painted nutmeg. The proposed revisions to refuse and recycling facility shall be submitted with the building permit application for review and approval by the Development of Community & Economic Development, Planning Division project manager.

**Critical Areas:** The site is located within Zone 1 and Zone 2 of the Wellfield Capture Zones and there are no other critical areas located onsite.

**Parking:** Retail sales use requires a minimum and maximum of 2.5 spaces per 100 square feet of net floor area. Drive-through facilities shall be so located that sufficient on-site vehicle stacking space is provided for the handling of motor vehicles using such facility during peak business hours. Typically 5 stacking spaces per window are required unless otherwise determined by the Planning Director. Stacking spaces cannot obstruct required parking spaces or ingress/egress within the site or extend into the public right-of-way.

The proposed retail building has a net area of 1,659 SF resulting in a min/max requirement of four (4) stalls. The applicant has proposed to provide eight (8) parking spaces and 2 air/water vacuum stalls, which exceeds the min/max required and permitted under a retail sales use parking requirement. The Master Site Plan considers Cedar River Station retail to be a shopping center use requiring a minimum of 0.4 spaces per 100 square feet of net floor area and a maximum of 0.5 per 100 square feet of net floor area, which would result in compliance with the parking standards (minimum requirement of 7 stalls and a maximum parking permitted of 8 stalls). Staff recommends approval of the parking modification for the retail sales use as it is consistent with the Master Site Plan and the decision criteria of a modification per RMC 4-9-250D.5.

The applicant has requested a modification from the stacking standards of five (5) spaces per vehicle fueling positions. Based on the minimum standards, the 12 fueling positions require five (5) cars stacking spaces. This would result in a total of 60 car stacking spaces. The applicant has requested to provide six (6) fueling drive lanes, each with two (2) fueling location with room for 12 car staking spaces.

The applicant submitted a modification request based on fueling transactions with “pay at pump” technology being very short term. Therefore, the turn-over times at fueling locations are short term, meaning there are locations constantly available. It is staff’s understanding that the applicant has considered multiple layouts, pump configurations, spacing/layout options and their impacts on queuing/stacking, and the most effective option is provided in the submitted plans. The proposal would provide six (6) stacking lanes serving 12 available vehicle fueling pumps (VFP) but would also maintain four (4) passing lanes, two (2) beneath the fuel canopy and one (1) each on the outer edge of the canopy.

The applicant has indicated that the proposed design allows drivers to maneuver to take advantage of an unused VFP, and maintain the maximum number of VFPs available. Furthermore, the applicant surmises that the combination of more refueling options and easier accessibility lending to decrease dwell time would equate to less of a chance of a queuing/stacking problem developing. The applicant concluded that the proposed on-site queuing area is sufficient to accommodate the expected demand and customers will not need to stack outside the fueling area to select a fueling location. Staff supports a modification to the stacking requirement as proposed as it would not be injurious to other property owners in the vicinity and would substantially implement the policy direction of the Comprehensive Plan, while meeting the objectives and safety functions as identified in the decision criteria found in RMC 4-9-250D.5.

Additionally, the provided site plan identifies enough room for six (6) car stacking spaces for the proposed drive-through car wash.

*The minimum amount of accessible parking stalls required for parking lots of eight (8) stalls is one (1).*

Based on the provided site plan, all parking stalls meet the minimum dimensional standards. The applicant has proposed to provide seven (7) standard stalls and one (1) ADA stall. All north/south drive aisles are proposed to be 23 feet in width. Staff recommends approval of the drive aisle width as proposed as they are consistent with the approved Master Site Plan modification. The 23 foot drive aisle would not impact site circulation and would provide sufficient back out space from the convenience store and vehicle circulation from car wash and fuel station at the site. In addition, the pedestrian connections identified throughout the site allow for sufficient vehicle/pedestrians separation, thereby reducing any conflicts that could result from vehicle turning movements.

Bicycle parking was identified on the site plan, immediately west of parking space #1. The code requirements found in RMC 4-4-080F.11 for bicycle parking are not a requirement of the development agreement. Bicycle parking is an appropriate amenity for the site.

***Pedestrian Access:*** *A pedestrian connection shall be provided from a public entrance to the street, in order to provide direct, clear and separate pedestrian walks from sidewalks to buildings entries and internally from buildings to abutting retail properties.*

Based on the provided Master Site Plan and Site Plan a pedestrian connection will be provided from the public entrance to the streets, in order to provide direct, clear and separate pedestrian walks from sidewalks to buildings entries and internally from buildings to Cedar River Station retail to the west (*Exhibit 3*).

**c. Development Agreement – Design Regulation Compliance and Consistency:**

Exhibit B of the Development Agreement related to the subject project, contains design standards (*Exhibit 19*). The intent is that any commercial development on either of these two parcels be of a quality that will fit in with its residential and nearby rural surrounds rather than being garish or “Disney-like” in its setting near the urban/rural Urban Growth Boundary. The intent, also, is that both parcels, if developed independently of each other, share common thematic elements such as building forms,

materials, signage, and landscaping to the intent reasonably practical in view of the ultimate uses on the parcels. As demonstrated in the table below, the proposal meets the intent of the Design Regulations on the basis of individual merit if all conditions of approval are met.

***i. Common Thematic Elements:** Building elements such as those identified below are to be used throughout each of the two parcels to create a unifying architectural statement for the development of each parcel and both parcels when seen from SR 169.*

|                                  |  |
|----------------------------------|--|
| ✓                                | <b>Materials:</b> Drawings submitted for review and approval shall identify exterior materials such as masonry or concrete block that will be used on the façades of all buildings.  |
| ✓                                | <b>Fenestration:</b> Drawings submitted for review and approval shall identify location of openings and types of glazing proposed, including color of glass and frames.  |
| ✓                                | <b>Roofing:</b> Drawings submitted for review and approval shall identify roofing style (flat, gabled, pitched, mansard, etc.), material, pitch, and color, and ensure that these are consistent throughout the development of each parcel.  |
| Partial Compliance With Standard | <p><b>Additional Architectural Elements:</b> Drawings submitted for review and approval shall include type and location of awnings (if any), and their proposed materials and color. All exterior lighting should be shown on all relevant elevations and perspectives. Glass and metal awnings (or awnings of other permanent materials) or overhanging eaves shall be provided on all façades visible from public streets.</p> <p><b>Other Architectural Embellishments (if any):</b> Drawings submitted for review and approval shall include decorative roof treatments, decorative lighting, decorative paneling, etc., which are encouraged and, if proposed, shall be shown in all relevant building elevations and perspectives.</p> <p><b>Staff Comment:</b> The following architectural elements and embellishments should be required in order to increase design regulation compliance and consistency Cedar River Station and the development agreement. Staff recommends as a condition of approval the elevation plan be resubmitted to include the following elements with the building permit application for review and approval by the Development of Community &amp; Economic Development, Planning Division project manager.</p> <p>The convenience store should add the following elements: one additional design feature to the south elevation that is consistent with the common thematic elements of the north elevation, add a window to the west elevation that is not blocked by the refuse and recycling facility, add a metal canopy with cable supports to the west elevation between the split face CMU columns, and add an awning or metal canopy with cable supports above the east elevation door.</p> <p>The car wash should complete the following elements: add a metal canopy with cable supports to the north and south elevations above the bay doors, add an awning or metal canopy with cable supports above the four west elevation windows, and add a window treatment to the four windows on the west elevation.</p> <p>The mechanical room should complete the following elements: add a metal canopy with cable supports to the south elevation above the two (2) entrances and add a window treatment to the cashier’s window on the west elevation.</p> |
| N/A                              | <b>Signage:</b> Drawings submitted for review and approval shall identify all proposed exterior signage including façade signs. Any allowed freestanding signs shall be ground-oriented monument type signs. Pole and roof top-mounted signs shall be prohibited.  |

|                                  |  |
|----------------------------------|--|
|                                  | <p><b>Staff Comment:</b> The provided elevations indicated that signage would be applied to the façade as wall signs. However, a specific signage package was not submitted with the application. All signs require a sign permit application; any proposed signs would be reviewed for consistency with the subject application at time of sign permit application.</p>   |
| ✓                                | <p><b>Paving Materials: Drawings submitted for review and approval shall identify all paving materials including driveways, parking area, and pathways.</b></p> <p><b>Staff Comment:</b> Around the convenience store, the applicant is proposing an 8-foot wide concrete apron, and a 5-wide concrete sidewalk extending north (towards SR 169) and west (towards Cedar River Station) from the concrete apron.</p>   |
| Partial Compliance With Standard | <p><b>Exterior Lighting: Drawings submitted for review and approval shall identify all proposed exterior lighting, including parking lot lighting, and decorative lighting along pedestrian corridors.</b></p> <p><b>Staff Comment:</b> Staff recommends as a condition of approval the final lighting plan be submitted with the building permit application for review and approval by the Development of Community &amp; Economic Development, Planning Division project manager.</p> |
| N/A                              | <p><b>Surface Water Detention Ponds: Surface water detention ponds (if any) shall be screened and landscaped with sight-obscuring evergreen plant materials. Where pond fencing is required, it shall be decorative in appearance and use permanent materials such as metal or decorative concrete block. Landscaping should buffer the exterior of all such fencing or walls.</b></p>   |

**d. Mitigation of impacts to surrounding properties and uses:**

City staff does not anticipate any adverse impact on surrounding properties and uses provided all conditions are complied with. The entire site would be landscaped along the perimeter with a minimum of 15 feet of landscaping in combination with fencing. The proposed fuel pumps are located along the street frontages away from the existing residential development to the south and east. The proposed site layout would not impair the use or enjoyment of surrounding uses and structures by the community. The nearby pedestrian connection provided to the apartments to the south would provide a walkable connection to retail services that are currently not provided today.

The proposed one-story structure would not be out of scale with the neighborhood; they are smaller in scale than many of the surrounding developments, including the apartments to the south and New Life Church @ Renton to the east. Any impacts of the building scale would be mitigated by the proposed landscaping along the street frontages and the modulations in the façades created by the various façade treatments.

The proposed refuse and recycling facility would be located approximately 58 feet north of the south property line. The applicant has provided screening details of this facility, which would include a 7.5 foot wall around three sides. The provided landscape plan identifies areas to the north and south of the facility with planter islands that will be planted with screening trees and plants.

The additional development of the site would have positive impacts that would far outweigh the potential negative impacts. The proposed project would improve a currently vacant un-vegetated site, which has already been graded for development. The new parking lot landscaping, the street frontage improvements along Maple Valley Highway (including curb, gutter, and sidewalk) and landscaping along the east and south property lines would improve the site.

**e. Mitigation of impacts of the proposed site plan to the site:**

The scale, height, and bulk of the proposed buildings are appropriate for the site, and are anticipated to be architecturally compatible with the existing development in the project vicinity. The convenience store, fuel canopy and car wash cover 24% of the site, which is less than the maximum building coverage permitted by code. The conceptual landscaping plan would provide for perimeter site landscaping around the site with the exception of areas for walkways and driveways. Parking lot landscaping is also proposed which would include trees, shrubs, and ground cover.

The applicant divided the development into three (3) separate above ground structures and two underground fuel tanks. The design and configuration of the site reduces the overall visual scale of the development and at the same time creates cohesiveness between the buildings. The development continues the village feel established by the three retail buildings to the west. The design treatments, as proposed or conditioned, will be of a quality that will fit in with its residential and nearby rural surrounding. The buildings are placed in a manner that facilitates walkability to the apartments and neighboring retail center. Pedestrian connections are provided along the north frontage of the site and connected throughout the development from building to building, through the adjoining parking lots and to the residential development to the south.

**f. Conservation of area-wide property values:**

The proposed development is expected to conserve and possibly increase property values in the vicinity of the site. The development of the site provides improvements to infrastructure, landscaping and lighting and additional local service opportunities.

**g. Safety and efficiency of vehicle and pedestrian circulation:**

Access to the site would be from Maple Valley Highway and 152nd Ave SE. Right-of-way improvements are proposed along Maple Valley Highway and currently exist along 152nd Ave SE. All improvements along Maple Valley Highway (SR 169) would be required to comply with Washington State Department of Transportation standards. The site plan provides sufficient parking spaces and stacking space for the drive-through; in addition, internal site circulation appears sufficient to accommodate the anticipated users.

See above, sub-section *b. Zoning Compliance and Consistency*.

**h. Provision of adequate light and air:**

The proposed building is designed appropriately to allow adequate light and air circulation to the buildings, the master site, and adjacent properties. The design of the structures would not result in excessive shading of the property. In addition, there is ample area surrounding the building to provide normal airflow.

According to code, parking lot lighting fixtures are to be non-glare and mounted no more than 25 feet above the ground. This is to help minimize the impact onto adjacent properties. Staff does not anticipate that exterior lighting would become an issue due to the siting of the building, provided code requirements are met. A general lighting plan was identified in the site plan and elevation plans that identifies four (4) area lights 20-feet in height at roughly the four corners of the lot (*Exhibit 3*), 13 recessed light fixtures on the underside of the canopy (*Exhibit 6*), blue LED rope light on the north elevation of the fuel canopy, and 35 wall mounted decorative light fixtures (*Exhibits 5 & 16*). Staff recommends as a condition of approval that a lighting plan with foot-candles be submitted with the building permit application for review and approval by the Development of Community & Economic Development, Planning Division project manager.

**i. Mitigation of noise, odors and other harmful or unhealthy conditions:**

Noise and odor impact would occur as a result of the proposed project. Noise impacts would be a result of commercial retail development, including operating noise and vehicular noise. Methods of attenuating car wash noise will be taken by the applicant to limit noise transmitted to be within legal limits. During construction, dust, controlled with watering as necessary. After construction, automobile exhausts. The development agreement limits the number of drive-through businesses that the site can operate; the proposed combined development would not exceed the restriction. The noise related to vehicular traffic is not anticipated to be more than a typical fuel station development and would not exceed the noise associated with SR 169. Limiting 24 hour operations of the car wash would limit the noise impacts of the project, which is adjacent to residential development. Long term operating hours for the car wash will be limited by the applicant to 7:00 AM to 9:00 PM.

**j. Availability of public services and facilities to accommodate the proposed use:**

Public services are currently available to the site. The project is within the Cedar River Water and Sewer District Service areas. There are storm drainage improvements in SR 169 and in 152nd Ave SE. The applicant has proposed a combined detention and water quality vault that will be constructed under the parking lot for Cedar River Station, which would connect to the existing storm drainage improvements (*Exhibit 12*). The applicant is proposed to provide frontage improvements along SR 169.

**k. Prevention of neighborhood deterioration and blight:**

The architectural design and landscaping of the site would ensure that the property would make a positive contribution to the physical condition and visual aesthetic of the area. No deterioration or blight is expected to occur as a result of this proposal. As long as design standards are maintained, the development would be compatible with the existing area.

**I. CONCLUSIONS:**

1. The proposal complies with the Plan Review Criteria if all conditions of approval are met.
2. The proposal is consistent with the approved master plan, LUA12-000193, if all conditions of approval are met.
3. The proposal is compliant and consistent with the plans, policies, regulations, design standards and approvals, as vested pursuant to the applicable Development Agreement, if all conditions of approval are met.
4. The proposed use is anticipated to be compatible with existing and future surrounding uses as permitted in the CA zoning classification and sufficiently mitigates the impact to surrounding properties and uses.
5. The proposed project is anticipated to conserve area-wide property values.
6. The proposed buildings are the appropriate scale, height and bulk for the site.
7. The proposed site plan ensures safe movement for vehicles and pedestrians and has mitigated potential effects on the surrounding area if all conditions of approval are complied with.
8. There are adequate public services and facilities to accommodate the proposed use.
9. Adequate parking and sufficient vehicular and pedestrian circulation for the proposed use has been provided if all conditions of approval are complied with.

10. The proposed development would not generate any long term harmful or unhealthy conditions. Potential noise, light and glare impacts from the proposed use have been evaluated and mitigated if all conditions of approval are complied with.
11. The proposed location would not result in the detrimental overconcentration of a particular use within the City or within the immediate area of the proposed use. The proposed location is suited for the proposed use.
12. Landscaping has been provided in all areas not occupied by buildings or paving. Additional landscaping has been provided in order to buffer adjacent properties from potentially adverse effects of the proposed use.

**J. DECISION:**

The proposed Site Plan, Parking Modification and Stacking Modification for Cedar River Chevron, File No. LUA14-000799, ECF, MOD, SA-A is **approved** subject to the following conditions:

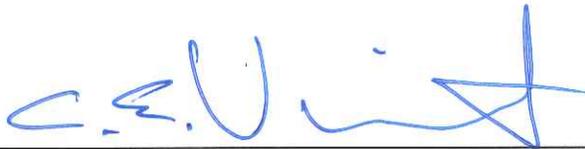
1. The applicant shall comply with the two mitigation measures issued as part of the Determination of Non-Significance-Mitigated, dated August 4, 2014 (*Exhibit 15*).
2. Internal cross walks shall be constructed of concrete. Materials and/or patterns shall be identified with the construction permit application. Final approval of materials and/or patterns shall be reviewed by the Current Planning Project Manager prior to construction permit issuance.
3. The developer of the subject buildings shall tie into the pedestrian connection established for Cedar River Station (on Lot A) and agree to not contest the construction of such a connection. In addition the developer shall be required to develop the connection that is of similar construction material and pattern.
4. The applicant shall provide a final landscape and irrigation plan with the building permit application for review for review and approval by the Current Planning Project Manager, which increases the landscaping in the east landscaping buffer in front of the car wash/mechanical building and includes compliance with the Development Agreement and consistency to Cedar River Station's landscaping plan prior to construction permit issuance.
5. The applicant shall submit a materials board that is consistent with the approved materials provided with the Cedar River Station, subject to the approval of the Current Planning Project Manager prior to building permit approval.
6. The applicant shall be required to provide a final lighting plan that adequately provides for public safety without casting excessive glare on adjacent properties, including a detail sheet with foot-candles, at the time of building permit review. The lighting plan shall be submitted to, and approved by, the Current Planning Project Manager prior to building permit approval.
7. The applicant shall revise the refuse and recycling facility to reduce the size from 300 SF to no greater than 144 SF in area and no less than 126 SF, while maintaining a gate opening of at least 12 feet wide for haulers. The colors shall be modified to match the convenience store colors by providing two (2) courses of CMU in mocha color followed by nine (9) courses of CMU in café au lait color. The roof color shall be nutmeg. The proposed revisions to refuse and recycling facility shall be submitted with the building permit application for review and approval by the Development of Community & Economic Development, Planning Division project manager.
8. The applicant shall revise the following building elevations as follows:
  - a. Convenience store:

- i. Add additional design features, such as windows, awnings, and/or glazing to the south elevation that is consistent with the common thematic elements of the north elevation.
  - ii. Add a window to the west elevation that is not blocked by the refuse and recycling facility.
  - iii. Add a metal canopy with cable supports to the west elevation between the split face CMU columns.
  - iv. Add an awning or metal canopy with cable supports above the east elevation door.
- b. Carwash:
- i. Add a metal canopy with cable supports to the north and south elevations above the bay doors.
  - ii. Add an awning or metal canopy with cable supports above the four west elevation windows.
  - iii. Add a window treatment to the four windows on the west elevation.
- c. Mechanical:
- i. Add a metal canopy with cable supports to the south elevation above the two (2) entrances.
  - ii. Add a window treatment to the cashier's window on the west elevation.
- d. Fueling Facility Overhead Canopy:
- i. On each canopy elevation, angle the 1'-6" tall metal panels from a vertical pitch to a minimum 6:12 pitch to a maximum 10:12 pitch.
  - ii. The CMU cladding around the 10" square steel columns shall be increased from 4' in height to at least 7'-4" in height.
  - iii. The colors shall be modified to match the convenience store colors by providing two (2) courses of CMU in mocha color followed by nine (9) courses of CMU in café au lait color.

Final approval of the building elevations shall be reviewed by the Current Planning Project Manager prior to construction permit issuance.

**DATE OF DECISION ON LAND USE ACTION:**

**SIGNATURE:**



**C.E. "Chip" Vincent, CED Administrator**

8/5/14  
**Date**

TRANSMITTED this 5<sup>th</sup> day of August, 2014 to the Contact/Applicant/Owner:

Contact:  
JP Athwal  
20207 105th Ave SE  
Kent, WA

Applicant:  
Roger Ollenburg  
Roger Ollenburg Architect  
17405 33rd Dr SE  
Bothell, WA 98012

Owner:  
Cedar River Station LLC  
15215 SE 272nd St #201  
Kent, WA 98042

TRANSMITTED this 5<sup>th</sup> day of August, 2014 to the Parties of Record:

Brian Gonzales  
15323 SE 155th Pl, V3  
Renton, WA 98058

TRANSMITTED this 5<sup>th</sup> day of August, 2014 to the following:

Jennifer Henning, Planning Director

Craig Burnell, Building Official

Steve Lee, Development Engineering Manager

Vanessa Dolbee, Current Planning Manager

Fire Marshal

Renton Reporter

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***Land Use Action Appeals, Request for Reconsideration, & Expiration***

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The Environmental Determination and the Administrative Site Development Plan Review decisions will become final if the decisions are not appealed within 14 days of the decision date.

**Environmental Determination Appeal:** Appeals of the environmental determination must be filed in writing to the Hearing Examiner on or before 5:00 p.m., August 22, 2014.

**Administrative Site Development Plan Approval Appeal:** Appeals of the administrative site development plan review decision must be filed in writing to the Hearing Examiner on or before 5:00 p.m., on August 22, 2014.

**APPEALS:** An appeal of the decision(s) must be filed within the 14-day appeal period (RCW 43.21.C.075(3); WAC 197-11-680). Renton Municipal Code Section 4-8-110 governs appeals to the Hearing Examiner. Appeals must be filed in writing together with the \$250.00 application fee to Hearing Examiner, City of Renton, 1055 South Grady Way, Renton, WA 98057. Additional information regarding the appeal process may be obtained from the City Clerk's Office, Renton City Hall - 7th Floor, (425) 430-6510.

**RECONSIDERATION:** Within 14 days of the decision date, any party may request that a decision be reopened by the Administrator (Decision-maker). The Administrator (Decision-maker) may modify his decision if material evidence not readily discoverable prior to the original decision is found or if he finds there was misrepresentation of fact. After review of the reconsideration request, if the Administrator (Decision-maker) finds sufficient evidence to amend the original decision, there will be no further extension of the appeal period. Any person wishing to take further action must file a formal appeal within the 14-day appeal timeframe.

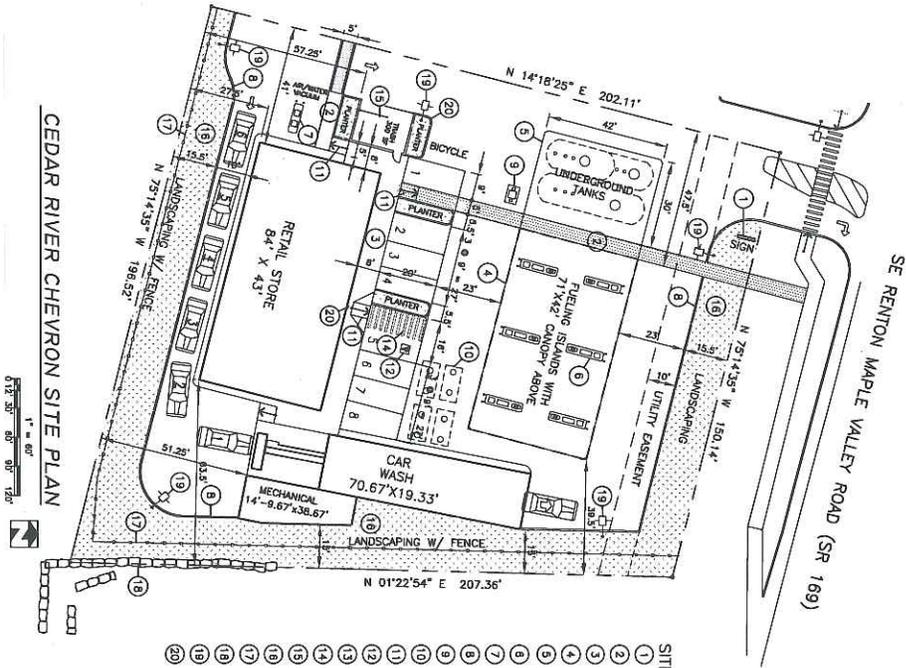
**EXPIRATION:** The Administrative Site Development Plan Review decision will expire two (2) years from the date of decision. A single two (2) year extension may be requested pursuant to RMC 4-9-200.

**THE APPEARANCE OF FAIRNESS DOCTRINE:** provides that no ex parte (private one-on-one) communications may occur concerning the land use decision. The Doctrine applies not only to the initial decision, but to Appeals to the Hearing Examiner as well. All communications after the decision/approval date must be made in writing through the Hearing Examiner. All communications are public record and this permits all interested parties to know the contents of the communication and would allow them to openly rebut the evidence in writing. Any violation of this doctrine could result in the invalidation of the appeal by the Court.



# EXHIBIT 3

## CEDAR RIVER CHEVRON SITE PLAN



- SITE PLAN NOTES**
- 1 CHEVRON SIGN AND BASE
  - 2 CONCRETE SIDEWALK
  - 3 CONCRETE APRON
  - 4 CONCRETE FUELING DRIVE SLAB
  - 5 CONCRETE SLAB OVER STORAGE TANKS
  - 6 CONCRETE FUELING ISLAND (TYPICAL)
  - 7 CONCRETE VACUUM ISLAND
  - 8 EXTRUDED CONCRETE CURB
  - 9 OIL/WATER SEPARATOR
  - 10 CAR WASH WATER RECLAIM TANKS
  - 11 ACCESSIBLE NON-SLIP RAMP
  - 12 ACCESSIBLE PAINTED SYMBOL
  - 13 PAINTED STRIPE 4"
  - 14 PAINTED STRIPE 8"
  - 15 TRASH ENCLOSURE
  - 16 LANDSCAPING
  - 17 WOOD FENCE 5' HIGH
  - 18 EXISTING ROCKERY
  - 19 AREA LIGHT 20" HIGH
  - 20 ACCESSIBLE PARKING SIGN

PRELIMINARY

SHEET: SP.1  
 SCALE AS NOTED  
 DATE: 10/20/2011  
 DRAWN BY: J. HARRIS  
 CHECKED BY: J. HARRIS  
 APPROVED BY: J. HARRIS

REVISIONS  
 NO. DATE DESCRIPTION

**NEW CHEVRON GAS STATION**  
 CEDAR RIVER STATION  
 15205 MAPLE VALLEY HIGHWAY  
 RENTON, WASHINGTON 98058  
**SITE PLANS**  
**PROJECT INFORMATION**  
THIS DOCUMENT IS THE PROPERTY OF ROGER OLLENBURG ARCHITECT. IT IS TO BE USED ONLY FOR THE PROJECT AND SITE SPECIFICALLY IDENTIFIED HEREON. IT IS NOT TO BE REPRODUCED, COPIED, OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, ELECTRONIC OR MECHANICAL, WITHOUT THE WRITTEN PERMISSION OF ROGER OLLENBURG ARCHITECT.

**Roger Ollenburg Architect**  
 Architectural & Planning Services  
 17405 33rd Drive SE  
 Bothell, WA 98012  
 206.947.2295  
 roger@architect.com

(Empty box for stamp or signature)

# PLANT SCHEDULE

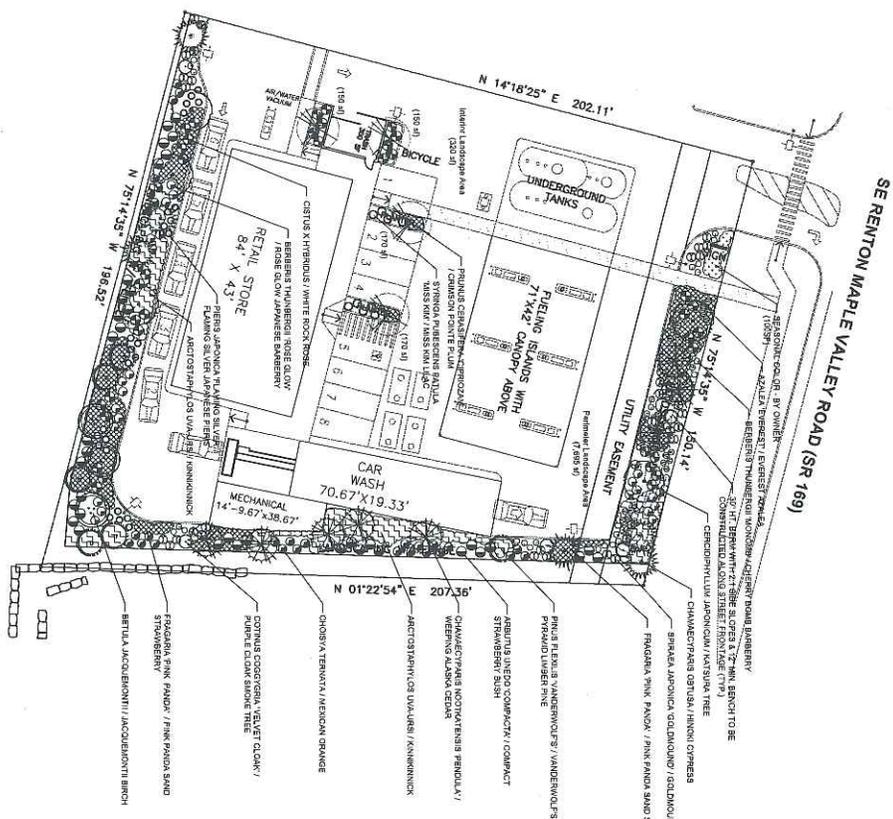
| SYMBOL                   | QTY.    | BOTANICAL NAME / COMMON NAME                                       | SIZE             | SPACING, CONDITION & REMARKS                  |
|--------------------------|---------|--|------------------|---|
| <b>TREES - DECIDUOUS</b> |         |  |                  |   |
| 3                        | 3       | BETULA ACCORDANTII / ACCORDANT BIRCH                               | 8-10' HEIGHT     | B&B MULTITRUNK (2 CANES), EVENTUALLY BRANCHED |
| 5                        | 5       | CERCOPHYLLUM JAPONICUM / JAPANESE TREE                             | 2' OAL           | B&B BRANCHED @ 8' HT. (SHADE TREE QUALITY)    |
| 4                        | 4       | PRUNUS CENSURENSIS CROZATA / CHAMISE POINT PINE                    | 2' OAL           | B&B BRANCHED @ 8' HT. (SHADE TREE QUALITY)    |
| <b>TREES - EVERGREEN</b> |         |  |                  |   |
| 6                        | 6       | CHAMAECYPARISS VICTORIANA / WEeping ALASKA CEDAR                   | 5-7' HEIGHT      | B&B NURSERY GROWN                             |
| 8                        | 8       | CHAMAECYPARISS OBTUSA / HANNO CYPRESS                              | 6-7' HEIGHT      | B&B NURSERY GROWN                             |
| 6                        | 6       | PRUNUS FLEXILIS VANDERWOLFFII / VANDERWOLFF'S PYRAMID LAMBERT PINE | 8-7' HEIGHT      | B&B NURSERY GROWN                             |
| <b>SHRUBS</b>            |         |  |                  |   |
| 37                       | 37      | ABUTILUS UNDO COMPACTUS / COMPACT STRAWBERRY BUSH                  | 5 GAL. CONTAINER | FULL FOLIAGE, 4" O.C.                         |
| 21                       | 21      | ZALEA EVERESTI / EVEREST ZALEA                                     | 2 GAL. CONTAINER | FULL FOLIAGE, 2" O.C.                         |
| 25                       | 25      | BEBERIS THUNDERBOLT MONSIEUR / CHERRY BOMB BARBERRY                | 2 GAL. CONTAINER | FULL FOLIAGE, 2" O.C.                         |
| 23                       | 23      | BEBERIS THUNDERBOLT ROSE GLOW / ROSE GLOW JAPANESE BARBERRY        | 2 GAL. CONTAINER | FULL FOLIAGE, 2" O.C.                         |
| 29                       | 29      | CHOISYA TENAXIA / MEXICAN ORANGE                                   | 5 GAL. CONTAINER | FULL FOLIAGE, 3" O.C.                         |
| 28                       | 28      | CERTUS X HYBRIDUS / WHITE ROSE ROSE                                | 5 GAL. CONTAINER | FULL FOLIAGE, 4" O.C.                         |
| 5                        | 5       | COTINUS COCOGRANA / VELVET CLOAK / VELVET CLOAK SMOKE TREE         | 5 GAL. CONTAINER | FULL FOLIAGE, 4" O.C.                         |
| 25                       | 25      | FERNIS JAPONICA / FLAMING SILVER / FLAMING SILVER JAPANESE PERSIA  | 5 GAL. CONTAINER | FULL FOLIAGE, 3" O.C.                         |
| 36                       | 36      | SPHALIA JAPONICA GOLDMOUND / GOLDMOUND SPHEA                       | 2 GAL. CONTAINER | FULL FOLIAGE, 3" O.C.                         |
| 10                       | 10      | SYNCHIZA PARSICENSIS / PATULA VIBIS KIVI / MISS KILL LAC           | 1 GAL. CONTAINER | FULL FOLIAGE, 3" O.C.                         |
| <b>ORNAMENTALS</b>       |         |  |                  |   |
| 1320 SF                  | 1320 SF | ACTINOTRYPHICUS / WALKER / KANAWANICK                              | 1 GAL. CONTAINER | 2" O.C.                                       |
| 1835 SF                  | 1835 SF | FRAGARIA PINK PINKO / PINK PINKO SAND STRAWBERRY                   | 4" POTS          | 1" O.C.                                       |
| 100 SF                   | 100 SF  | SEASONAL COLOR / 3" OWENS  | 4" POTS          | 1" O.C.                                       |

## SITE CALCULATIONS

Perimeter Landscape Area = 7,695 sf  
 Interior Landscape Area = 320 sf  
 Total Landscape Area Provided = 8,034 sf

CITY REVIEW - NOT FOR CONSTRUCTION

## Landscape Plan



## EXHIBIT 4

# LANDSCAPE PLAN & SCHEDULE

## CHEVRON GAS STATION AT CEDAR RIVER STATION

Roger Ollenburg, Architect  
 15255 Maple Valley Highway, Renton WA 98056

**BRADLEY DESIGN GROUP, INC.**  
 LANDSCAPE ARCHITECTURE + SITE PLANNING

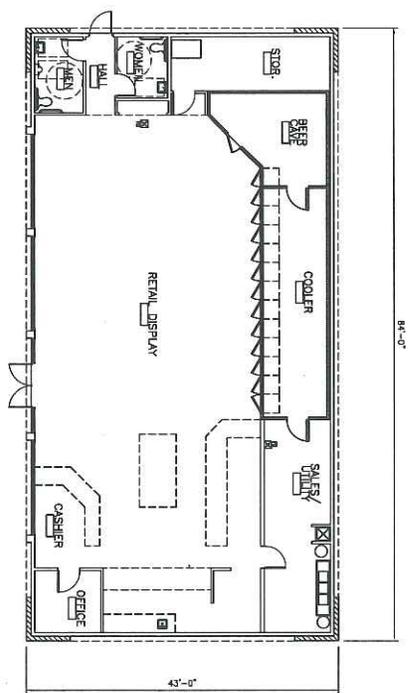
4558 24th Street  
 Tacoma, WA 98402  
 1.253.272.4848  
 1.253.276.0172  
 www.bradleydesign.com  
 info@bradleydesign.com

| NO. | REVISION     | DATE | BY |
|-----|--------------|------|----|
| 1   | 17 June 2014 | KBR  |    |

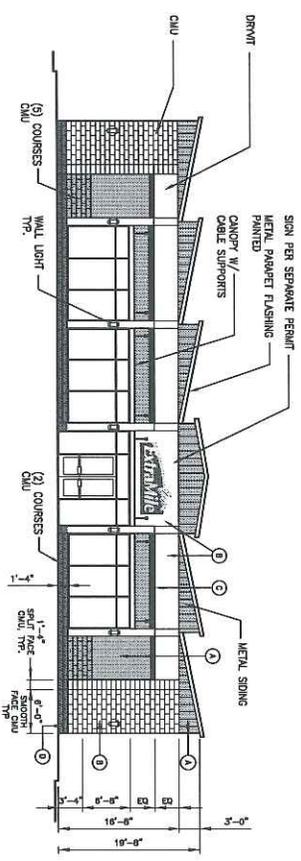
Rev to Plans per City Comments

17 May 2014  
 2 Revs  
 Roger Ollenburg, Architect

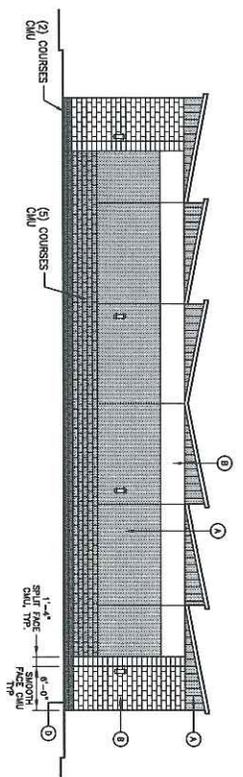




**FLOOR PLAN**  
1/8" = 1'-0"

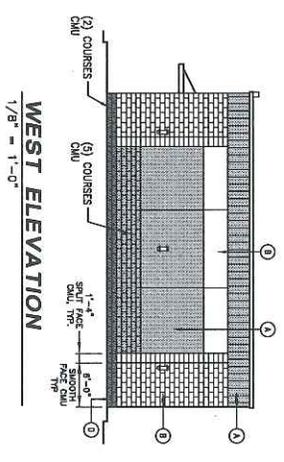


**NORTH ELEVATION**  
1/8" = 1'-0"

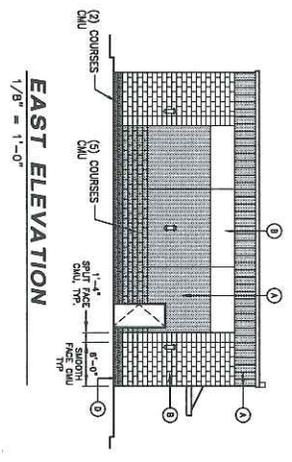


**SOUTH ELEVATION**  
1/8" = 1'-0"

- LEGEND**
- ① NUNING
  - ② ONE A/J LIT
  - ③ CHILLED WINE
  - ④ NOCHA



**WEST ELEVATION**  
1/8" = 1'-0"



**EAST ELEVATION**  
1/8" = 1'-0"

**EXHIBIT 5**

PRELIMINARY

sheet: A2.0  
SCALE AS SHOWN  
DATE: 3/17/2011

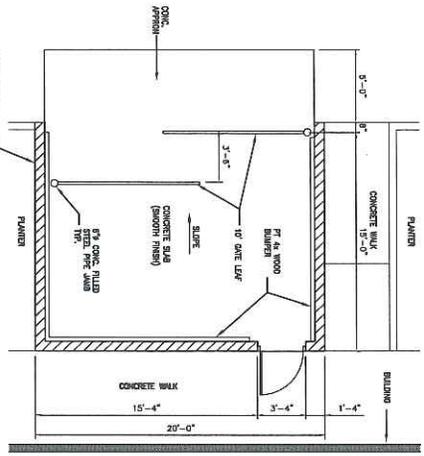
REVISIONS

**NEW CHEVRON GAS STATION**  
CEDAR RIVER STATION  
16355 MAPLE VALLEY HIGHWAY  
BENTON, WASHINGTON 99005

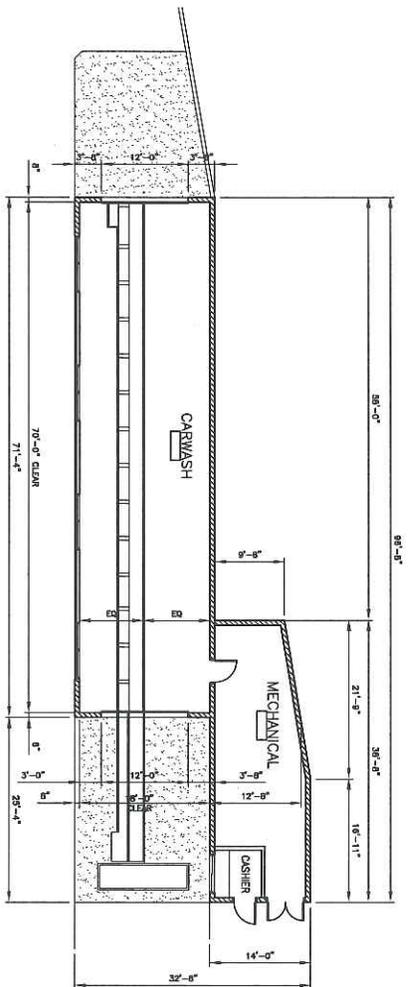
**EXTRA MILE**  
**FLOOR PLAN & ELEVATIONS**

**Roger Ollenburg-Architect**  
Architectural and Planning Services  
17405 33rd Drive SE  
Bothell, WA 98012  
206.947.2295  
Fax: 425.486.8022 rogero.architect@comcast.net

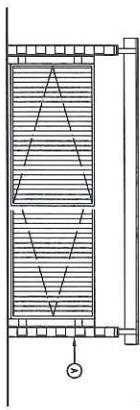
1770 REGISTERED ARCHITECT  
ROGER V. OLLENBURG  
STATE OF WASHINGTON



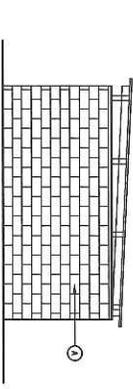
TRASH ENCLOSURE PLAN  
1/4" = 1'-0"



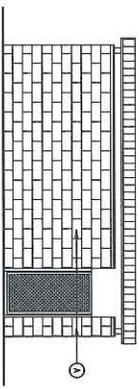
CARWASH PLAN  
1/8" = 1'-0"



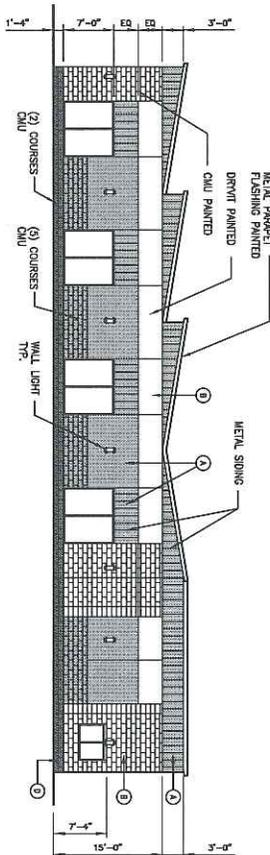
WEST ELEVATION  
1/4" = 1'-0"



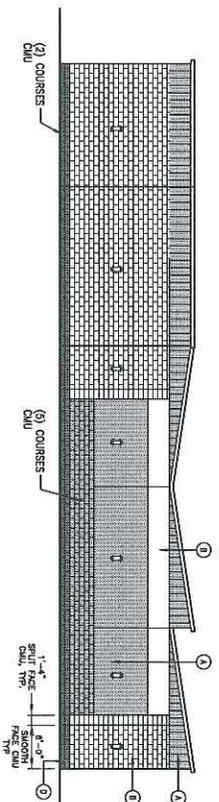
SOUTH ELEVATION (NO. OH)  
1/4" = 1'-0"



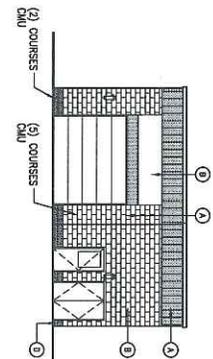
EAST ELEVATION  
1/4" = 1'-0"



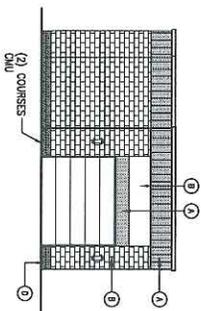
WEST ELEVATION  
1/8" = 1'-0"



EAST ELEVATION  
1/8" = 1'-0"



SOUTH ELEVATION  
1/8" = 1'-0"



NORTH ELEVATION  
1/8" = 1'-0"

- COLORS
- 1 NOMIG
  - 2 CAPE AU LAT
  - 3 CHILLED WINE
  - 4 MIOCHA

PRELIMINARY

Sheet: A&O

DATE: 3/17/2011

SCALE: AS NOTED

REVISIONS

| NO. | DATE | DESCRIPTION |
|-----|------|-------------|
|     |      |             |

**NEW CHEVRON GAS STATION**  
CEDAR RIVER STATION  
15365 MAPLE VALLEY HIGHWAY  
BENTON, WASHINGTON 99008

**CARWASH & TRASH ENCLOSURE PLANS & ELEVATIONS**

**Roger Ollenburg Architect**  
Architectural and Planning Services  
17405 33rd Drive SE  
Bothell, WA 98012  
206.947.2295  
206.947.2295  
roa@roaarchitect.com

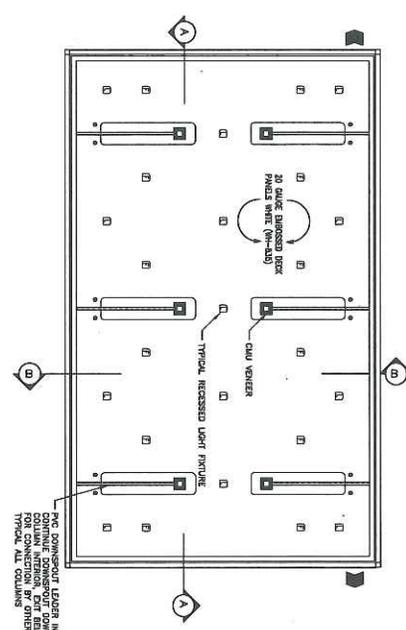
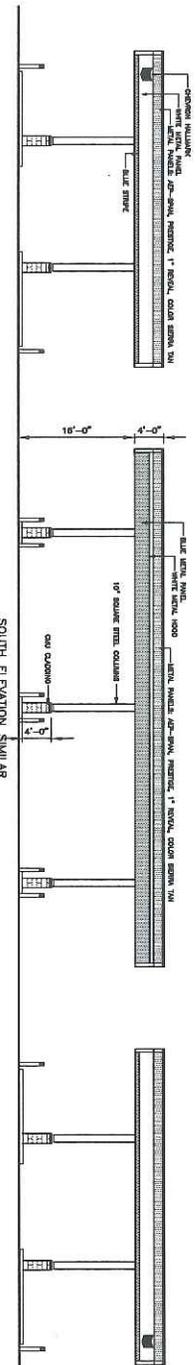
1730 REGISTERED ARCHITECT  
ROGER V. OLLENBURG  
STATE OF WASHINGTON

WEST

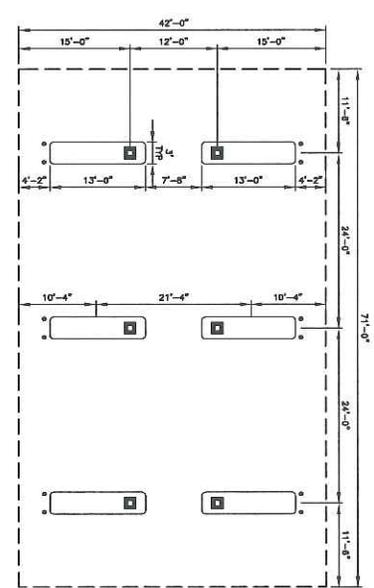
NORTH

EAST

CANOPY ELEVATIONS  
1/8" = 1'-0"



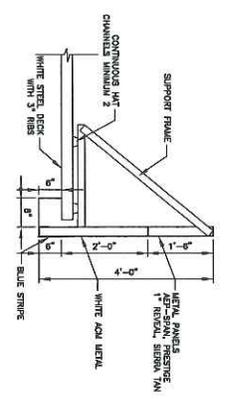
REFLECTED CEILING PLAN  
1/8" = 1'-0"



CANOPY AND ISLANDS PLAN  
1/8" = 1'-0"

- GENERAL NOTES:
1. THIS CANOPY IS CONSTRUCTED OF:
    - a.) LIGHT GAUGE PREFINISHED STEEL PANELS
    - b.) LIGHT GAUGE PREFINISHED ALUMINUM COMPOSITE PANELS
    - c.) PRIME COATED STRUCTURAL STEEL SECTIONS
    - d.) CONCRETE MASONRY UNITS COLUMN CLADDING

A FASCIA SIDE SECTION  
3/4" = 1'-0"



B FASCIA FRONT SECTION  
3/4" = 1'-0"

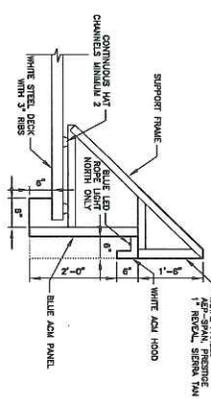


EXHIBIT 6

PRELIMINARY SHEET: A6.0

REVISIONS:

NEW CHEVRON GAS STATION  
CEDAR RIVER STATION  
15255 MAPLE VALLEY HIGHWAY  
RENTON, WASHINGTON 98055

GAS CANOPY  
PLANS, ELEVATIONS, DETAILS

Roger Ollenburg Architect  
Architectural & Planning Services  
17405 3rd Drive SE  
Bothell, WA 98012  
206.947.2295  
roger.architect@comcast.net

REGISTERED ARCHITECT  
ROGER V. OLLENBURG  
STATE OF WASHINGTON

**GEOTECHNICAL ENGINEERING STUDY  
CEDAR RIVER STATION  
15221 RENTON MAPLE VALLEY ROAD  
KING COUNTY, WASHINGTON**

**October 1, 2007  
Project No. E-9060-12**

**Prepared for  
Eagle Creek Land and Development, LLC  
13701 Southeast 253rd Street  
Kent, Washington 98042**



**Entire Document  
Available Upon Request**

**EXHIBIT 7**

**H CONSULTING INCORPORATED**  
1805 136th Place Northeast  
Suite 201  
Bellevue, Washington 98005  
(425) 643-3780  
Toll Free 1-888-739-6670

9802 29<sup>th</sup> Ave W #B102  
Everett, WA 98204  
425-356-3372  
425-356-3374 fax

## E3RA

November 22, 2013  
E13081

Jaspinder "JP" Athwal  
20207 105<sup>th</sup> Avenue SE  
Kent, WA 98031

Subject: **Geotechnical Engineering Report**  
Cedar River Chevron  
15355 Maple Valley Highway  
Renton, Washington

Dear Mr. Athwal:

At your request, an E3RA engineer visited the site of the proposed Cedar River Chevron to be located at 15355 Maple Valley Highway in Renton, Washington. The purpose of our visit was to excavate geotechnical test pits as part of conducting a review of the geotechnical report titled "Geotechnical Engineering Study, Cedar River Station, 15221 Renton Maple Valley Road, King County, Washington" submitted by Earth Consulting Incorporated (ECI) on October 1, 2007. We understand that the City of Renton has required this work as a site specific geotechnical investigation for the Cedar River Chevron project.

The site is on an approximately 3/4-acre parcel in the City of Renton in King County, Washington. Based on the preliminary discussions, development of the property for a gas station is anticipated. Grading will involve cuts and fills of less than 3 feet on the relatively level property. An excavation to between 8 and 10 feet will be made for underground fuel storage tanks. As shown on the site plan, test pit 1 (TP-1) is located just northeast of the proposed fuel tanks and test pit 2 (TP-2) is located in the southern portion of the site, west of the proposed retail building.

We excavated the two test pits on-site using a backhoe provided by your contractor. The test pits were excavated to a depth of approximately 9 feet. Dark brown, moist, medium dense silty sand with some gravel (SM), transitioning to brown with trace gravel after 3 feet, was encountered in the top 7.5 feet of TP-1. The remaining 1.5 feet consisted of brown, slightly moist, medium dense, fine to medium sand (SP). TP-2 encountered the same material, with silty sand (SM) from 0 to 4 feet, sand (SP) from 4 to 7 feet, and sand with gravel (SP) from 7 to 9 feet. No groundwater or test pit caving was encountered in either test pit. ECI excavated two test pits in close proximity to the proposed station. In these they encountered silty sand or sand with silt in the upper 5.5 to 7.5 feet, underlain by silty or poorly graded gravel.

**Entire Document  
Available Upon Request**

**EXHIBIT 8**



October 10, 2013

City of Renton  
Planning Division  
1055 South Grady Way  
Renton, WA 98057

**RE: Cedar River Station, Fueling Station  
Site Plan Review Drainage Letter**

Parcel: 232305-9211  
Address: 15355 Maple Valley HWY, Renton WA 98058  
Site Area: 35,031 sf (0.80 acres)  
Zoning: CA (Commercial Arterial)

#### **Project Description**

The proposal is to construct a fueling station with retail shop and automated carwash. This project will be constructed on Lot B of the Cedar River Station development.

The Cedar River Station project is a retail center consisting of 3 commercial buildings on one parcel (Lot A) and a fueling station on a second parcel (Lot B), totaling approximately 3 acres. The Cedar River Station development was recently approved for construction by the City of Renton. Engineering plans and a drainage report for Cedar River Station have been prepared by ESM Consulting Engineers (ESM). The drainage report prepared by ESM addresses permanent drainage management for both Lot A and B. A copy of this report prepared by ESM has been included with this letter.

The scope of the Fueling Station project is the fueling station and car wash on Lot B of Cedar River station. This letter is to reference the drainage report prepared by ESM.

CONTACT  
phone: 253-301-4157  
fax: 253-336-3950  
london@beylerconsulting.com  
beylerconsulting.com

OFFICE  
10314 100<sup>th</sup> St. SW  
Lakewood, WA 98498

Plan. Design. Manage

CIVIL ENGINEERING | LAND SURVEY  
PROJECT MANAGEMENT | PERMIT EXI

**Entire Document  
Available Upon Request**

**EXHIBIT 9**

# CEDAR RIVER STATION TECHNICAL INFORMATION REPORT

FEBRUARY 28, 2013

Prepared for

Cedar River Station, LLC  
15215 SE 272<sup>nd</sup> Street, Suite 201  
Kent, WA 98042



Submitted by

ESM Consulting Engineers, LLC  
33400 8<sup>th</sup> Avenue South, Suite 205  
Federal Way, WA 98003

253.838.6113 tel  
253.838.7104 fax



[www.esmcivil.com](http://www.esmcivil.com)

Entire Document  
Available Upon Request

Job No. 1320-005-008

**EXHIBIT 9**

## **NEW CHEVRON GAS STATION**

### **CONSTRUCTION MITIGATION**

Proposed construction dates: Start September 15, 2014; Completion March 1, 2015

Construction hours and days:

Monday through Friday 7:00 AM - 8:00 PM

Saturday 9:00 AM – 8:00 PM

Sunday Not Allowed

Hauling hours and days:

Monday through Friday 8:30 AM – 3:30 PM

Saturday and Sunday Not Allowed

Transportation: SE Renton Maple Valley Road

Mitigation measures: Dust controlled with cobble truck entrance and watering.

Special hours for construction or hauling: None proposed at this time.

Traffic control plan: None proposed at this time.

---

**TRAFFIC IMPACT ANALYSIS**

**For**

**CEDAR RIVER STATION**

August 23, 2012

Prepared by:

Gary A. Norris, PE, PTOE  
DN Traffic Consultants  
PO Box 547  
Preston, WA 98050  
(425)765-5721

Client:

Eagle Creek Land Development LLC

**Entire Document  
Available Upon Request**

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**EXHIBIT 11**

---





## NOTICE OF APPLICATION AND PROPOSED DETERMINATION OF NON-SIGNIFICANCE-MITIGATED (DNS-M)

A Master Application has been filed and accepted with the Department of Community & Economic Development (CED) – Planning Division of the City of Renton. The following briefly describes the application and the necessary Public Approvals.

**DATE OF NOTICE OF APPLICATION:** July 3, 2014  
**LAND USE NUMBER:** LUA14-000799, ECF, MOD, SA-A  
**PROJECT NAME:** Cedar River Chevron

**PROJECT DESCRIPTION:** The applicant is requesting approval of a fuel station (Cedar River Chevron) with a convenience store and car wash located in the Cedar River Station development at 15355 Maple Valley Highway (APN 232305-9211) on the south side of Maple Valley Highway (SR 169) and east of 152nd Avenue SE. The project site is a 0.8 acre (35,031 square feet) parcel and has a land use of Commercial Corridor (CC) and a zoning designation of Commercial Arterial (CA). The property is also located within Urban Design District 'D', and therefore subject to additional design elements.

In addition to a fuel station with underground storage tanks, the site will include a canopy with six double-sided pumps, a convenience store, and an automatic tunnel carwash. Primary access to the site is proposed off of Maple Valley Highway. The City of Renton's Critical Areas maps indicate the presence of seismic hazards related to potential liquefaction of soils during an earthquake event. There is an existing development agreement for the subject property. There are no trees on site. The proposal includes SEPA review, parking modification, and a vehicle stacking modification to enter the fueling area. The submittal included construction mitigation, geotechnical report, drainage report, and traffic study.

**PROJECT LOCATION:** 15355 SE Maple Valley Highway

**OPTIONAL DETERMINATION OF NON-SIGNIFICANCE, MITIGATED (DNS-M):** As the Lead Agency, the City of Renton has determined that significant environmental impacts are unlikely to result from the proposed project. Therefore, as permitted under the RCW 43.21C.110, the City of Renton is using the Optional DNS-M process to give notice that a DNS-M is likely to be issued. Comment periods for the project and the proposed DNS-M are integrated into a single comment period. There will be no comment period following the issuance of the Threshold Determination of Non-Significance-Mitigated (DNS-M). A 14-day appeal period will follow the issuance of the DNS-M.

**PERMIT APPLICATION DATE:** June 20, 2014  
**NOTICE OF COMPLETE APPLICATION:** July 3, 2014  
**APPLICANT/PROJECT CONTACT PERSON:** Roger Ollenburg, Architect  
17405 33rd Drive SE; Bothell, WA 98012  
Email: rogero.architect@comcast.net  
**Permits/Review Requested:** Environmental (SEPA) Review, Administrative Site Plan Review, Parking Modification and Vehicle Stacking Modification  
**Other Permits which may be required:** Building and Construction Permits  
**Requested Studies:** Technical Information Report, Geotechnical Report, Drainage Report, and Traffic Study

If you would like to be made a party of record to receive further information on this proposed project, complete this form and return to: City of Renton, CED – Planning Division, 1055 So. Grady Way, Renton, WA 98057.

Name/File No.: Cedar River Chevron/LUA14-000799

NAME: \_\_\_\_\_  
MAILING ADDRESS: \_\_\_\_\_  
TELEPHONE NO.: \_\_\_\_\_



**ENVIRONMENTAL (SEPA) DETERMINATION OF  
NON-SIGNIFICANCE- MITIGATED (DNS-M)**

PROJECT NUMBER: LUA14-000799  
 APPLICANT: JP Athwal, Summit Petroleum Group  
 PROJECT NAME: Cedar River Chevron

PROJECT DESCRIPTION: The applicant is requesting Administrative Site Plan Review and Environmental Review, for the construction of a new Chevron Fuel Station. The proposal includes a convenience store (3,612 SF), car wash (1,801 SF), and fueling facility under an overhead canopy (2,982 SF) with six double-sided pumps and two underground fuel storage tanks (20,000 gallon capacity), along with associated parking, landscaping and infrastructure improvements. The development proposal is associated with the previously approved Cedar River Station project to the west and will use the existing storm water system. The 0.80 acre site is located within the Commercial Corridor (CC) land use designation and Commercial Arterial (CA) zoning classification. The subject property is located on the south side of Maple Valley Highway (SR 169). Primary access to the site is proposed off of Maple Valley Highway and secondary access is from 152nd Avenue SE. A total of eight (8) parking spaces are proposed. The site is located within Zone 1 and Zone 2 of the Wellfield Capture Zones and there are no other critical areas located onsite.

PROJECT LOCATION: 15355 Maple Valley Highway, State Route 169  
 LEAD AGENCY: City of Renton  
 Environmental Review Committee  
 Department of Community & Economic Development

The City of Renton Environmental Review Committee has determined that it does not have a probable significant adverse impact on the environment. An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(c). Conditions were imposed as mitigation measures by the Environmental Review Committee under their authority of Section 4-9-070D Renton Municipal Code. These conditions are necessary to mitigate environmental impacts identified during the environmental review process. Because other agencies of jurisdiction may be involved, the lead agency will not act on this proposal for fourteen (14) days.

**Appeals of the environmental determination must be filed in writing on or before 5:00 p.m. on August 22, 2014.** Appeals must be filed in writing together with the required fee with: Hearing Examiner, City of Renton, 1055 South Grady Way, Renton, WA 98057. Appeals to the Examiner are governed by RMC 4-8-110 and more information may be obtained from the Renton City Clerk's Office, (425) 430-6510.

PUBLICATION DATE: August 8, 2014  
 DATE OF DECISION: August 4, 2014

SIGNATURES:

|  |               |   |               |
|--|---------------|---|---------------|
| _____<br>Gregg Zimmerman, Administrator<br>Public Works Department         | _____<br>Date | _____<br>Mark Peterson, Administrator<br>Fire & Emergency Services                      | _____<br>Date |
| _____<br>Terry Higashiyama, Administrator<br>Community Services Department | _____<br>Date | _____<br>C.E. "Chip" Vincent, Administrator<br>Department of Community &<br>Development | _____<br>Date |

Lighting Products

- Pole Mounted Luminaire
- Wall Mounted Luminaires
- Illuminating Bollard
- Catenary Suspended Luminaires
- Illuminating Columns
- Pendant Luminaires
- Recessed Wall Luminaires
- LED Tile / Strip
- In-ground Luminaires
- Illuminated Bench
- Floodlights
- Ceiling Mounted Luminaires

Site Amenities

Product Search

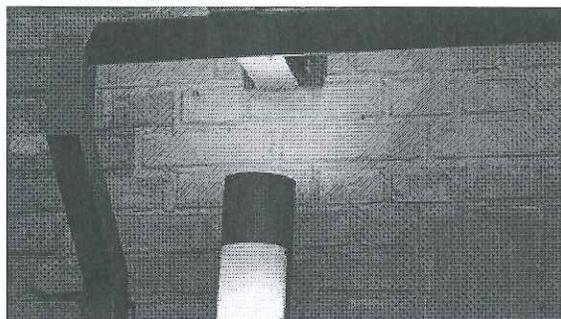
New Products

City Elements

Product overview

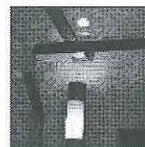
### VARELLO 485

#### Possible configurations

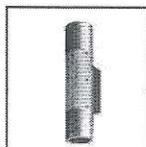


Nava DE - Berlin

#### References



#### Select your desired model variant



VARELLO 485

The simple cylindrical form of VARELLO blends subtly with a broad range of architecture. The heavy wall extruded aluminum housing, fitted top and bottom with precision-machined lens rings, provides subtle direct and indirect illumination. Matte acrylic window highlights the luminaire and serves as an illuminated focal point. Top and bottom lenses are clear tempered glass. Luminaire is finished in finely textured paint. Hardware is stainless steel. Standard color: matte silver grey metallic. Special colors available.

Specification

Documents

IES / Image

#### VARELLO 485 HOUSING

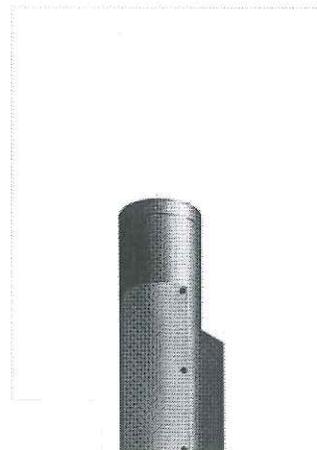
Cylindrical housing is machined from 6060 extruded aluminum alloy tubing. Semi-cylindrical front lens fits flush to the housing. Matte acrylic lens is sealed to the housing with one-piece closed cell silicone gasket and secured with six captive flathead countersunk screws. Upper and lower lens rings are precision machined from 6060 aluminum and sealed to the housing with one-piece diecut closed cell silicone gaskets. Upper and lower tempered glass lenses are sealed to the lens rings with one-piece closed cell silicone gasket. Luminaire with cast aluminum bracket mounts to wall surface with factory-supplied wall anchors and hardware. All hardware is stainless steel.

#### OPTICS

Vertical compact fluorescent lamp provides soft diffused illumination through front lens. Upper and lower EISA compliant PAR30 IRC halogen lamps provide symmetrical flood distribution.

#### LAMPING

Compact Fluorescent: PL-L 18W/4P  
 2 x 39PAR30S/IRC+/F/1 25 120V  
 2 x 55PAR30S/IRC+/F



**M E M O R A N D U M**

**DATE:** July 29, 2014  
**TO:** Clark H. Close, Associate Planner  
**FROM:** Steve Lee, Development Engineering Manager  
**SUBJECT:** Traffic Concurrency Test for the Cedar River Chevron;  
 File No. LUA14-000799, ECF, MOD, SA-A

The proposed Cedar River Chevron consists of a convenience store (3,612 sf), car wash (1,801 sf), and fueling facility under an overhead canopy (2,982 sf) with six double-sided pumps, along with associated underground fuel storage tanks, parking, landscaping and infrastructure. The 0.80 acre site is located at 15355 Maple Valley Highway (APN 232305-9211) with primary access proposed off of Maple Valley Highway and secondary access is from 152nd Avenue SE. The development proposal is associated with the previously approved Cedar River Station project to the west. The calculated average weekday daily trips, for Cedar River Chevron was estimated at 1,953 using the average weekday daily trip generation rate. Cedar River Station, as a single use, is estimated to generate 5,898 daily, 486 AM peak and 359 PM peak hour trips. The entire Cedar River Station, including the fuel station, is estimated to generate 105 PM peak trips (47 of which are attributed to Cedar River Chevron) on the adjacent arterial network with 50 inbound and 55 outbound. The project passes the City of Renton Traffic Concurrency Test per RMC 4-6-070.D as follows:

| <b>Traffic Concurrency Test Criteria</b>                      | <b>Pass?</b> |
|---|--------------|
| Implementation of citywide Transportation Plan?               | Yes          |
| Within allowed growth levels?                                 | Yes          |
| Project subject to transportation mitigation or impact fees?  | Yes          |
| Site specific street improvements to be completed by project? | Yes          |
| <b>Traffic Concurrency Test Passes</b>                        |              |

**Evaluation of Test Criteria**

Implementation of citywide Transportation Plan?: As shown on the attached citywide traffic concurrency summary, the city's investment in completion of the forecast traffic improvements are at 130% of the scheduled expenditure through 2013.

Within allowed growth levels?: As shown on the attached citywide traffic concurrency summary, the calculated citywide trip capacity for concurrency with the city adopted model for 2014 is 96,940 trips, which provides sufficient capacity to accommodate the 47

additional trips from this project. The resulting available traffic trips remaining is 96,893 trips.

Project subject to transportation mitigation or impact fees?: The project will be subject to transportation impact fees at time of building permit.

Development agreement (recording number 20070307000134)?: The Development Agreement includes a traffic mitigation fee credit. As identified in Section 4 of the Development Agreement, in 2003 the signalized intersection and turn lanes and other improvements were constructed with private funds totaling \$337,066. It was identified that the commercial site (subject site) would contribute an estimated 75 percent share of the traffic trips to the intersection. Based on this analysis, the City agreed that a sum of \$252,799.50 would be credited against the Renton traffic mitigation fees (now impact fees) due upon development of the property. Cedar River Chevron would be eligible to use any of the remaining transportation balance not already used by Cedar River Station up until the sum is expended.

Site specific street improvements to be completed by project?: The project will be required to complete all internal and frontage street improvements for the plat prior to recording. Any additional off-site improvements identified through SEPA or land use approval will also be completed prior to final occupancy.

### **Background Information on Traffic Concurrency Test for Renton**

The City of Renton Traffic Concurrency requirements for proposed development projects are covered under Renton Municipal Code (RMC) 4-6-070. The specific concurrency test requirement is covered in RMC 4-6-070.D, which is listed for reference:

#### ***D. CONCURRENCY REVIEW PROCESS:***

***1. Test Required:*** A concurrency test shall be conducted by the Department for each nonexempt development activity. The concurrency test shall determine consistency with the adopted Citywide Level of Service Index and Concurrency Management System established in the Transportation Element of the Renton Comprehensive Plan, according to rules and procedures established by the Department. The Department shall issue an initial concurrency test result describing the outcome of the concurrency test.

***2. Written Finding Required:*** Prior to approval of any nonexempt development activity permit application, a written finding of concurrency shall be made by the City as part of the development permit approval. The finding of concurrency shall be made by the decision maker with the authority to approve the accompanying development permits required for a development activity. A written finding of concurrency shall apply only to the specific land uses, densities, intensities, and development project described in the application and development permit.

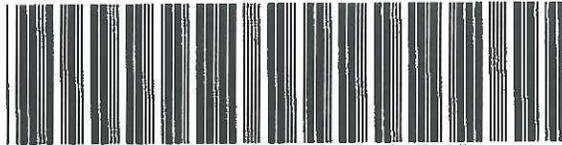
**3. Failure of Test:** *If no reconsideration is requested, or if upon reconsideration a project fails the concurrency test, the project application shall be denied by the decision maker with the authority to approve the accompanying development activity permit application.*

The Concurrency Management System established in the Transportation Element on page XI-65 of the Comprehensive Plan states the following:

*Based upon the test of the citywide Transportation Plan, consideration of growth levels included in the LOS-tested Transportation Plan, payment of a Transportation Mitigation Fee, and an application of site specific mitigation, development will have met City of Renton concurrency requirements.*

**Return Address:**

City Clerk's Office  
City of Renton  
1055 S. Grady Way  
Renton, Washington 98055



**20070307000134**

CITY OF RENTON AG  
PAGE 001 OF 014  
03/07/2007 09:22  
KING COUNTY, WA

45.00

Please print or type information

**Document Title(s)** (or transactions contained therein): (all areas applicable to your document **must** be filled in)

1. DEVELOPMENT AGREEMENT

**Reference Number(s) of Related Documents:** N/A

**Grantor(s)** (Last name, first name, initials)

- 1. AQUA BARN RANCH, INC., a Washington corporation
- 2.

Additional names on page \_\_\_\_\_ of document.

**Grantee(s)** (Last name first, then first name and initials)

- 1. CITY OF RENTON, a municipal corporation of the State of Washington

Additional names on page \_\_\_\_\_ of document.

**Legal description** (abbreviated: i.e. lot, block, plat or section, township, range)

LOTS 3 AND 4 OF KING COUNTY SHORT PLAT NUMBER L99S3019 (AFN 20010831900002)

Additional legals are on Page 1 of document.

**Assessor's Property Tax Parcel/Account Number**

Assessor Tax # not yet assigned

2323059210 and 2323059211

The Auditor/Recorder will rely on the information provided on the form. The staff will not read the document to verify the accuracy or completeness of the indexing information provided herein.

Entire Document  
Available Upon Request

**EXHIBIT 18**

**EXHIBIT B**

**AQUA BARN SITE CA ZONE DEVELOPMENT AGREEMENT DESIGN  
STANDARDS FOR COMMERCIAL DEVELOPMENT**

**Purpose:**

The intent of the following design standards is to set forth the desired character of future commercial development on the two visually prominent abutting parcels of land totaling approximate three (3) acres and fronting SR 169 on a portion of the former Aqua Barn site. The intent is that any commercial development on either of these two parcels be of a quality that will fit in with its residential and nearby rural surroundings rather than being garish or "Disney-like" in its setting near the urban/rural Urban Growth Boundary. The intent, also, is that both parcels, if developed independently of each other, share common thematic elements such as building forms, materials, signage, and landscaping to the extent reasonably practical in view of the ultimate uses on the parcels. Unless otherwise specified herein, all other relevant code requirements set forth in Title IV shall be met.

**Standards:**

1. **Site Master Plan:** A site plan for either or both of the two parcels at the time of development shall include information on building type, location, phasing (if any), etc.
  - a. **Buildings:** All proposed buildings shall be identified as to type, size, use, and location.
  - b. **Parking:** All proposed parking areas shall be identified as to location, number of stalls, location of drive aisles, points of ingress and egress, lighting, proposed landscaping and pedestrian walkways related to them.
  - c. **Open Spaces:** Common open spaces (if any) and their locations shall be identified, including open spaces such as larger landscape areas, storm retention ponds, etc.
  - d. **Pedestrian Features:** Proposed pedestrian walkways shall comply with RMC 4-3-040F.1.e and shall be identified as to location, materials, and what they are connecting or linked to.
2. **Common Thematic Elements:** Building elements such as those identified below are to be used throughout each of the two parcels to create a unifying architectural statement for the development of each parcel and both parcels when seen from the Renton – Maple Valley Highway (SR 169).

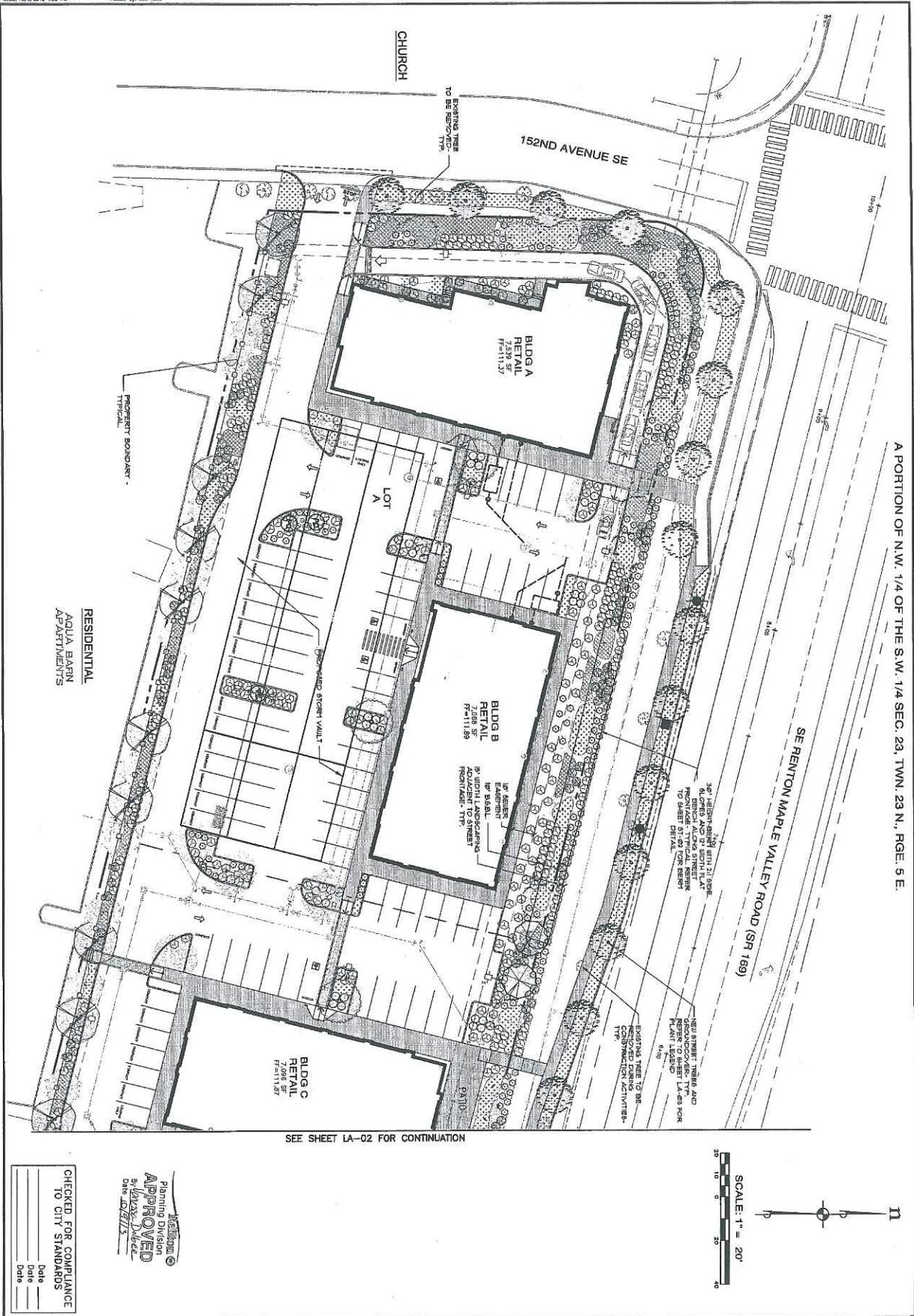
- a. Materials: Drawings submitted for review and approval<sup>a</sup> shall identify exterior materials such as masonry or concrete block that will be used on the façades of all buildings;
  - b. Fenestration: Drawings submitted for review and approval shall identify location of openings and types of glazing proposed, including color of glass and frames;
  - c. Roofing: Drawings submitted for review and approval shall identify roofing style (flat, gabled, pitched, mansard, etc.), material, pitch, and color, and ensure that these are consistent throughout the development of each parcel;
  - d. Additional Architectural Elements: Drawings submitted for review and approval shall include type and location of awnings (if any), their proposed materials and color, and all exterior lighting should be shown on all relevant elevations and perspectives. Glass and metal awnings (or awnings of other permanent materials) or overhanging eaves shall be provided on all facades visible from public streets.
  - e. Other Architectural Embellishments (if any): Drawings submitted for review and approval shall include decorative roof treatments, decorative lighting, decorative paneling, etc., which are encouraged and, if proposed, shall be shown in all relevant building elevations and perspectives.
  - f. Signage: Drawings submitted for review and approval shall identify all proposed exterior signage including façade signs. Any allowed freestanding signs shall be ground-oriented monument type signs. Pole and roof-top-mounted signs shall be prohibited.
3. Landscaping Along Street Frontages: Landscaping along abutting street frontages shall comply with the following provisions [provisions that are derived from the portion of the table in RMC 4-3-040D (Development Standards for Uses Located within the Renton Automall Areas A and B) concerning “Landscaping – Street Frontage Landscaping Requirements” for “All Uses in Area A, Dealerships and Related Uses in Area B”]:
- a. 15-Foot Wide Landscape Strip Required: A 15-foot wide landscape strip shall be required along all street frontages. This is in lieu of requirements in Chapter 4-2 RMC. Unimproved portions of abutting street right-of-way can be used in

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<sup>a</sup> Once “[d]rawings submitted for review and approval” have been approved by the Renton Development Services Division, unless the approval is appealed and the approval decision is modified, any development(s) actually constructed shall be consistent with the approved drawings unless drawing revisions consistent with this Exhibit B have been reviewed and approved by the Development Services Division. Notwithstanding RMC 4-9-200D.2.a.ii, no building façade modifications (such as the location of entrances/exits, changes in materials, fenestration, roofing, additional architectural elements or signage or aesthetic alterations) that relate to the subject matter of this Exhibit B shall be made without the approval of the Development Services Division.

combination with abutting private property to meet the required 15-foot landscape strip width.

- b. Street Tree Requirements: Unless the existing trees within the 15-foot wide landscape strip are retained, the landscaping provided in the 15-foot wide landscape strip shall include a minimum 30-inch high berm and 2½ inch caliper red maples (*Acer roburum*) planted 25 feet on center.
4. Landscape Materials: Common landscape elements shall be used throughout each of the two parcels to create unifying statement for the development of each parcel.
  - a. Plant Materials: Drawings submitted for review and approval shall identify all proposed living plant materials of a permanent nature including species and size at time of planting.
  - b. Paving Materials: Drawings submitted for review and approval shall identify all paving materials including driveways, parking areas, and pathways. In regard to buildings greater than 5,000 square feet in size, a minimum 8-foot wide concrete sidewalk with decorative banding shall be provided along the building side(s) abutting parking areas. Such sidewalks shall be raised from the grade of the abutting parking areas a minimum of four inches except for ramps for handicapped access and rolling of shopping carts.
  - c. Exterior Lighting: Drawings submitted for review and approval shall identify all proposed exterior lighting, including parking lot lighting, and decorative lighting along pedestrian corridors.
  - d. Fencing: A double-faced and stained 5-foot high wood fence shall be installed along any unfenced boundaries of the site that abut residential-zoned property. In order to improve pedestrian access to and from abutting residential properties, openings in the fence shall be provided (not more than one along the south boundary of each of Lots 3 and 4 of King County Short Plat Number L99S3019 and not more than one along the east boundary of said Lot 4).
5. Surface Water Detention Ponds: Surface water detention ponds (if any) shall be screened and landscaped with sight-obscuring evergreen plant materials. Where pond fencing is required, it shall be decorative in appearance and use permanent materials such as metal or decorative concrete block. Landscaping should buffer the exterior of all such fencing or walls.

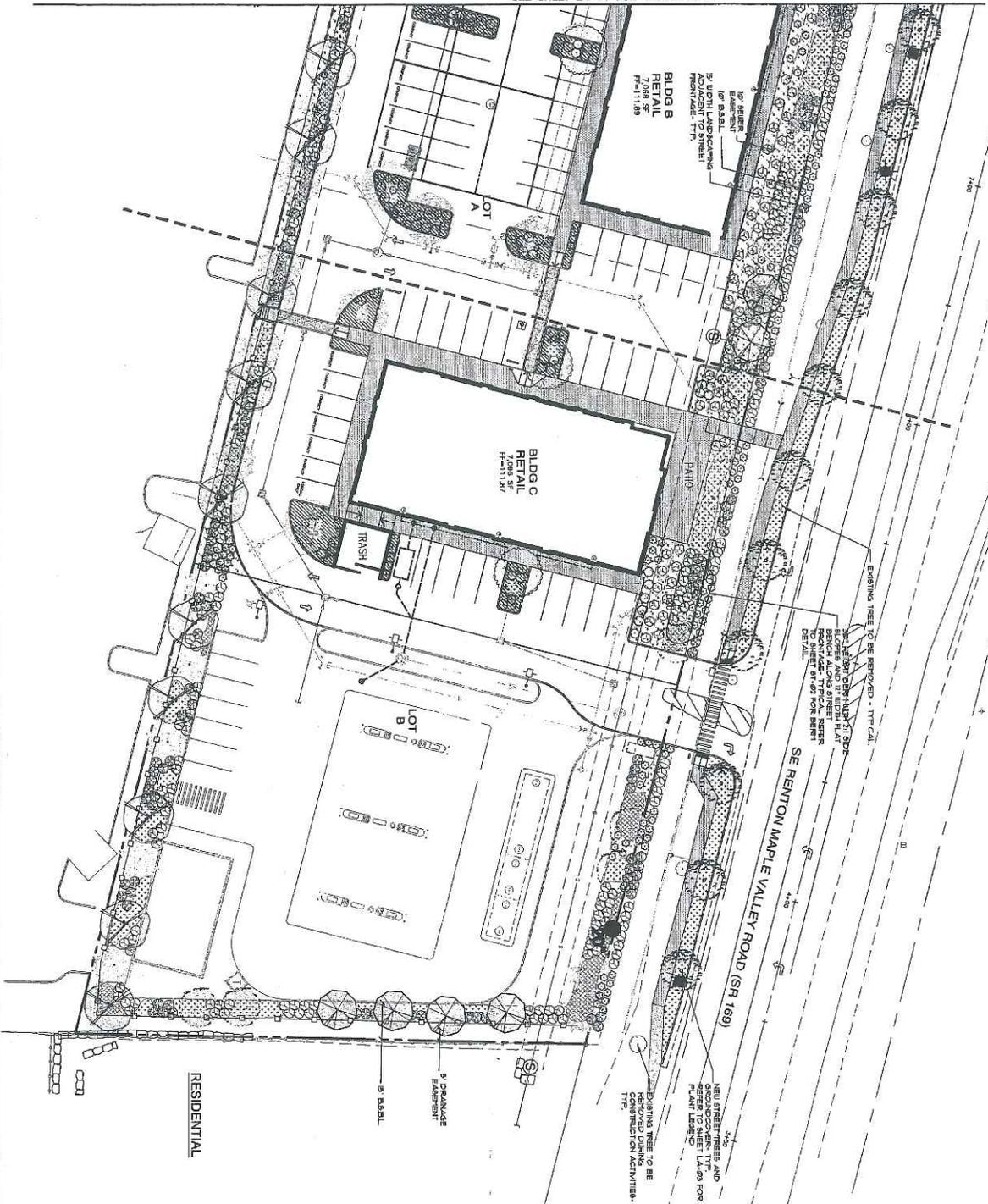


A PORTION OF N.W. 1/4 OF THE S.W. 1/4 SEC. 23, T.W.N. 23 N., R.G.E. 5 E.

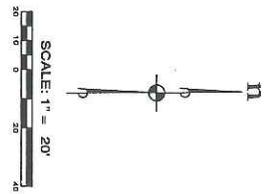
**EXHIBIT 20**

|                          |   |   |  |                                   |                                      |   |         |
|--------------------------|---|---|--|-----------------------------------|--------------------------------------|---|---------|
| 14 of 17 SHEETS<br>LA-01 | CEDAR RIVER STATION LLC<br>15215 SE 272ND STREET SUITE 201 KENT, WA 98042 RANDY GOODWIN | CONTACT: RANDY GOODWIN<br>33420 9TH AVE S, SUITE 205<br>FEDERAL WAY, WA 98003 | CONSULTING ENGINEERS, LLC<br>33420 9TH AVE S, SUITE 205<br>FEDERAL WAY, WA 98003 | CIVIL ENGINEERING<br>PUBLIC WORKS | LAND SURVEYING<br>PROJECT MANAGEMENT | LAND PLANNING<br>LANDSCAPE ARCHITECTURE | 10/1/13 |
|                          | CITY OF RENTON  | LANDSCAPE PLAN  | WASHINGTON   | REVISIONS                         |                                      |   |         |

SEE SHEET LA-01 FOR CONTINUATION



A PORTION OF N.W. 1/4 OF THE S.W. 1/4 SEC. 23, T.W.N. 23 N., R.G.E. 5 E.



CHECKED FOR COMPLIANCE  
 TO CITY STANDARDS  
 Date \_\_\_\_\_  
 Date \_\_\_\_\_  
 Date \_\_\_\_\_

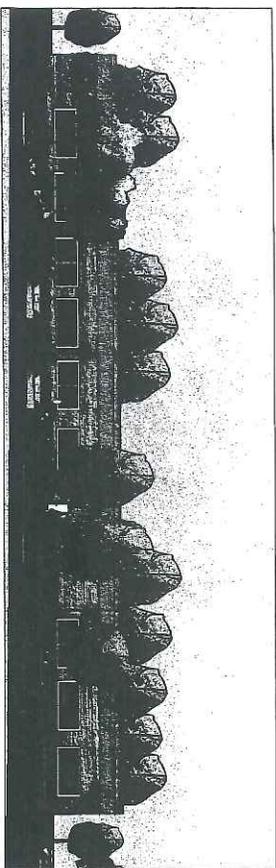
|   |  |  |  |   |   |   |  |                |
|---|--|--|--|---|---|---|--|----------------|
| <p>LA-02<br/>                 15 of 17 sheets</p> | <p>JOB NO.: 1200-001-002<br/>                 DATE: 06/26/2013<br/>                 DRAWN BY: JAH<br/>                 CHECKED BY: JAH<br/>                 DATE: 06/26/2013</p> | <p>CONTACT:<br/>                 RANDY GOODWIN</p> | <p>ESM CONSULTING ENGINEERS, L.L.C.<br/>                 35400 8th Ave. S., Suite 200<br/>                 Federal Way, WA 98003<br/>                 www.esmcivil.com</p> | <p>City of Renton<br/>                 LANDSCAPE PLAN<br/>                 WASHINGTON</p> | <p>Club Engineering<br/>                 Public Works</p> | <p>Land Surveying<br/>                 Project Management</p> | <p>Land Planning<br/>                 Landscape Architecture</p> | <p>10/1/13</p> |
|   | <p><b>CEDAR RIVER STATION LLC</b><br/>                 15215 SE 272ND STREET SUITE 201 KENT, WA 98042</p> <p><b>CEDAR RIVER STATION</b><br/>                 LANDSCAPE PLAN</p>  |  |  |   |   |   |  |                |

**PLANT SCHEDULE**

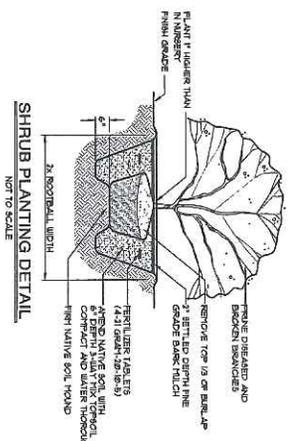
| SYMBOL          | SCIENTIFIC / COMMON NAME                   | QUANTITY | SIZE        | CORNER       |
|-----------------|--|----------|-------------|--------------|
| DECIDUOUS TREES |  |          |             |              |
| 1               | Populus tremuloides / QUAKING ASPEN        | 20       | 1 1/2" CAL. | B1B          |
| 2               | Nyssa sylvatica / BLACK WALNUT             | 10       | 1 1/2" CAL. | B1B          |
| 3               | Prunus emarginata / BITTER CHERRY          | 6        | 1 1/2" CAL. | B1B          |
| 4               | Betula papyrifera / PAPER BIRCH            | 5        | 1 1/2" CAL. | B1B          |
| 5               | Acer spicatum / WHITE MAPLE                | 10       | 1 1/2" CAL. | B1B          |
| 6               | Fraxinus latifolia / OREGON ASH            | 15       | 1 1/2" CAL. | B1B          |
| 7               | Prunus canadensis / SLOPE PINE             | 31       | 6" HT MIN   | B1B          |
| 8               | Physalis peruviana / OREGON BOX            | 17       | 8" GALLON   | MIN. 10" HT. |
| 9               | Opuntia densiflora / SUDANSE PINEAPPLE     | 14       | 8" GALLON   | MIN. 10" HT. |
| 10              | Thymus occidentalis / PACIFIC LUX THYME    | 304      | 1 1/2" CAL. | B1B          |
| 11              | Philadelphus lewisii / THORN ORANGE        | 152      | 1 1/2" CAL. | B1B          |
| 12              | Ribes sanguineum / FLORENSIA CURRANT       | 224      | 1 1/2" CAL. | B1B          |
| 13              | Physocarpus opulifolius / PACIFIC NINEBARK | 3        | 1 1/2" CAL. | B1B          |
| 14              | Physalis elaeagnifolia / WILD STRAWBERRY   | 1        | 1 GALLON    |              |
| 15              | Groundcover                                |          |             |              |
| 16              | Plant groundcover w/ 1/2" gravel           |          |             |              |
| 17              | Plant groundcover w/ 1/2" gravel           |          |             |              |

NOTE: SOME FERTILIZER PLANTING AREAS DO NOT SHOW A GROUND COVER SYMBOL. THIS IS BECAUSE ALL PLANTING AREAS WILL RECEIVE FERTILIZER PLANTING AREAS.

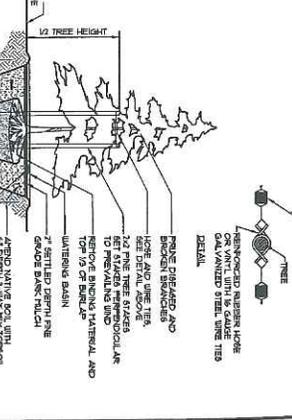
PLANT GROUND COVER W/ 1/2" GRAVEL PATTERNS AS SHOWN IN ALL PLANTING AREAS. ALL OTHER AREAS WILL BE COVERED WITHIN THREE YEARS.



**SITE ELEVATION**  
N19



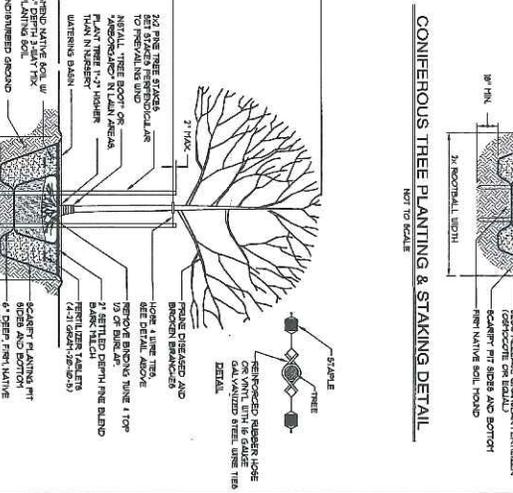
**SHRUB PLANTING DETAIL**  
NOT TO SCALE



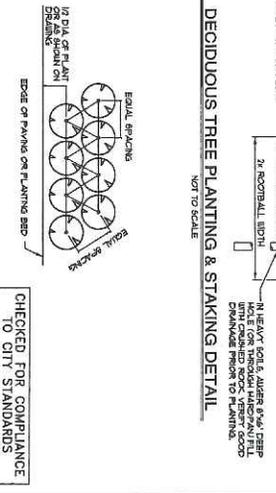
**LANDSCAPE NOTES**

1. CONTRACTOR IS RESPONSIBLE TO REVIEW PLAN SET AND CLARIFY ANY QUESTIONS PRIOR TO BEGINNING.
2. DO NOT DIG BEFORE LOCATING UTILITIES.
3. SUBGRADE, INCLUDING BERMS, TO WITHIN 1/4" FOOT PROVIDED BY GENERAL CONTRACTOR UNLESS OTHERWISE NOTED.
4. SUBGRADE SHALL BE RECLAIMED OR NOTIFIED IF CONDITIONS REQUIRE.
5. APPLY 6" DEPTH TYPICAL 3/4" MAX TOPSOIL IN ALL PLANTING AREAS OR AS OTHERWISE NOTED.
6. ALL PLANTING AREAS SHALL BE STAKED TO INDICATE ALL PLANTING AREAS. STAKES TO BE SET AT 10" ABOVE GRADE. STAKES OR TANNIN AND SHALL NOT CONTAIN ANY CORROSIVE DETERMINANT TO PLANT GROWTH.
7. CONTRACTOR IS RESPONSIBLE FOR BRUSHING ANY PROPOSED SUBSTITUTIONS OR LATER SEASON FIELD CONDITIONS TO THE ATTENTION OF THE LANDSCAPE ARCHITECT.
8. LANDSCAPE ARCHITECT HAS ADJUSTED PLANT QUANTITIES OR VARIETIES BASED ON AVAILABILITY OR FIELD CONDITIONS.
9. ALL PLANT MATERIAL SHALL BE DISEASE FREE AND AGREE IN A VEGEATION GROWING CONDITION.
10. THE TREE SHALL BE THE SAME SPECIES AS SHOWN FOR NUMBER STOCK LISTED ABOVE.
11. ALL PLANTING SHALL HAVE TRUNK GUARD SPACING, BRUSH HAIR PLANTING AND 1/4" EDGE BEAM FIRST ROW WITHIN 1" OF MOUND EDGE.
12. ALL PLANTING ARE CONTIGUOUS THROUGHOUT PLANTING BED. IF NO SYMBOL IS SHOWN, REFER TO PLAN NOTE AND/OR PLANT LEGEND.
13. ALL PLANTING SHALL BE PLANTING TO PLAN NOTE SYMBOL'S SHOWN ON PLAN.
14. ALL TREES SHALL BE WATERED TO MAINTAIN PROPER PLANTING AREAS. WATERING SHALL BE HANDLED BY THE CONTRACTOR. WATERING SHALL BE HANDLED BY THE CONTRACTOR. WATERING SHALL BE HANDLED BY THE CONTRACTOR.
15. STREET TREES SHALL HAVE UNIFORM BRANCHING HEIGHT OF 9' TO 10' MINIMUM ABOVE GRADE OR AS OTHERWISE NOTED.
16. STREET TREES NEAREST INTERSECTIONS SHALL CONFORM TO LOCAL SIGN DISTANCE REQUIREMENTS.
17. STREET TREE LOCATIONS ARE APPROXIMATE AND SHOULD BE ADJUSTED PER STREET LIGHT AND UTILITY LOCATIONS.
18. CONTRACTOR AGREES TO MAINTAIN AND MAINTAIN ALL PLANT MATERIAL FOR ONE FULL GROWING SEASON POST INSTALLATION AND SHALL REPLACE DEAD PLANT MATERIAL.
19. CONTRACTOR SHALL MAINTAIN AND MAINTAIN ALL PLANT MATERIAL FOR ONE FULL GROWING SEASON POST INSTALLATION TO THE SATISFACTION OF THE ARCHITECT AND/OR OWNER.
20. ALL TREES SHALL BE STAKED AFTER ONE FULL GROWING SEASON. TREES THAT HAVE DIED WITHIN THE ONE YEAR WARRANTY / MAINTENANCE PERIOD SHALL BE REPLACED AND BE STAKED PER THE DETAIL.
21. LANDSCAPE CONTRACTOR SHALL MAINTAIN SITE UNTIL FINAL INSPECTION AND ACCEPTANCE BY AUTHORIZING AGENCY AND / OR OWNER.

**CONIFEROUS TREE PLANTING & STAKING DETAIL**  
NOT TO SCALE



**DECIDUOUS TREE PLANTING & STAKING DETAIL**  
NOT TO SCALE



**SHRUB AND GROUND COVER SPACING DETAIL**  
NOT TO SCALE

CHECKED FOR COMPLIANCE TO CITY STANDARDS

Date: \_\_\_\_\_

Date: \_\_\_\_\_

**CEDAR RIVER STATION LLC**  
 15215 SE 272ND STREET SUITE 201 KENT, WA 98042  
 CONTACT: RANDY GOODWIN

**ESM CONSULTING ENGINEERS, LLC**  
 1111 1st Avenue, Suite 100  
 Federal Way, WA 98003

**CEDAR RIVER STATION**  
 LANDSCAPE DETAILS & NOTES

CITY OF RENTON WASHINGTON

www.esmcivil.com

Civil Engineering  
 Land Surveying  
 Public Works

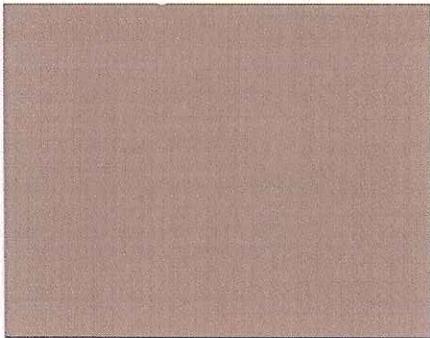
Land Planning  
 Project Management  
 Landscape Architecture

DATE: 10/2/2012  
 TIME: 1:41 PM  
 USER: jrt-hls

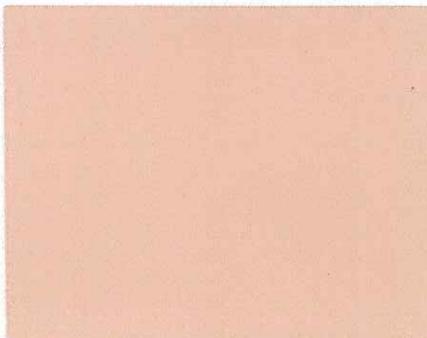
16 of 17 SHEETS



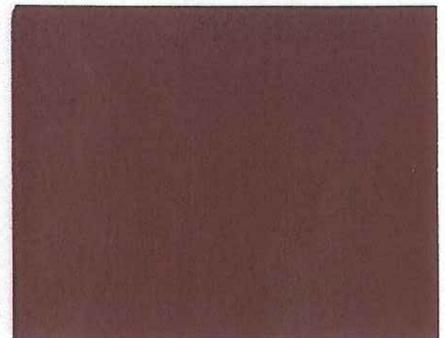
**EXTERIOR COLORS**



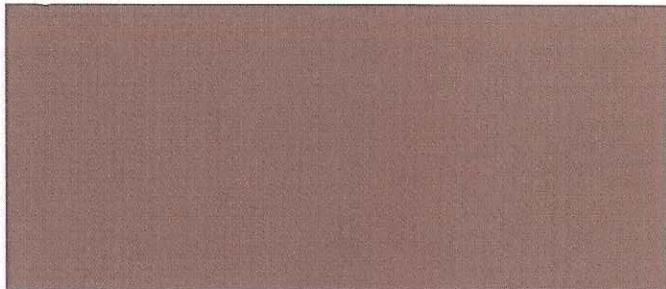
**Nutmeg**



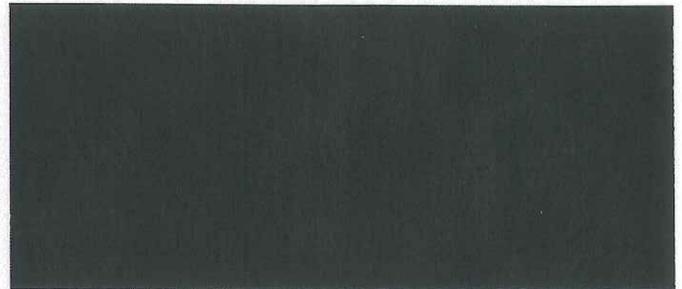
**Café Au Lait**



**Chilled Wine**

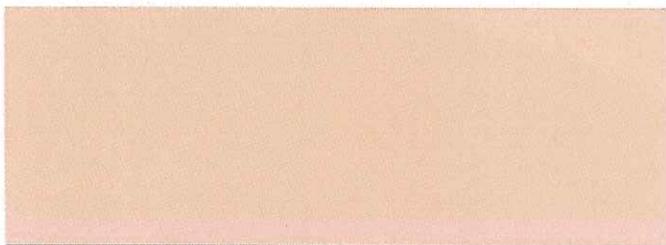


**Mocha**

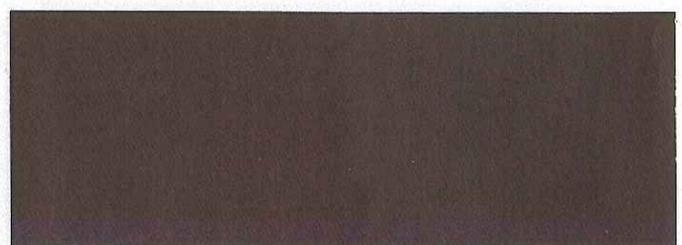


**Noir**

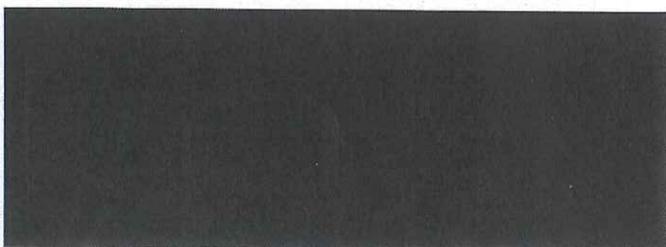
**INTERIOR COLORS**



**Interactive Cream**



**Sable**



**Darkroom**



**Noir**

Note: Tu

**EXHIBIT 21**

information

# CHEVRON

## Hallmark 21 Image Refresh



**JONES-BLAIR**<sup>®</sup>  
INDUSTRIAL COATINGS  
PAINT CARD

### Image Refresh Colors (Canopy Fascia, Canopy Columns, Etc.)

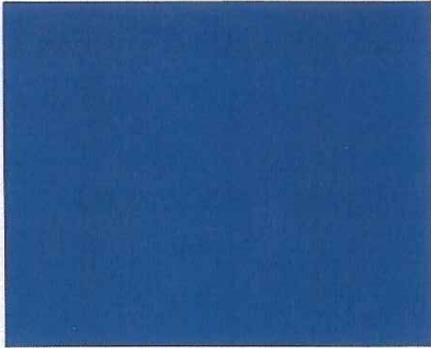


Image Refresh (IR) Blue

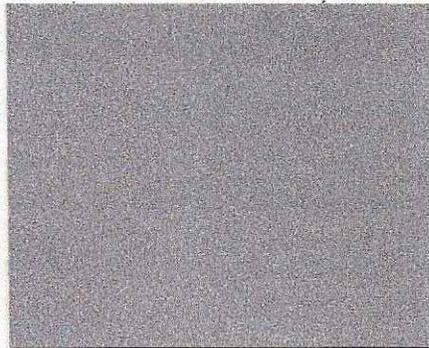


Image Refresh (IR) Silver

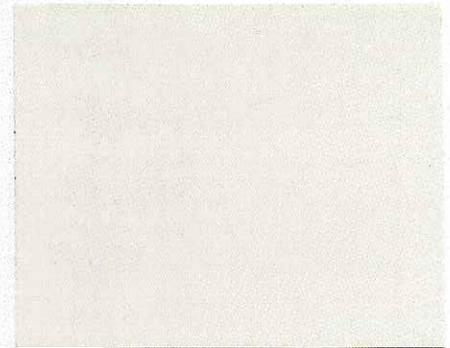
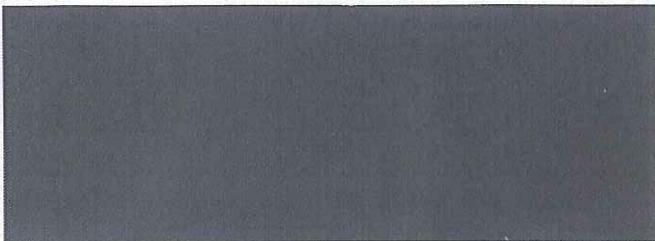


Image Refresh (IR) White

### HM21 Colors - Canopy Under Decks, Bollards, Sign Poles, Island Forms, Directional Signs & Poles

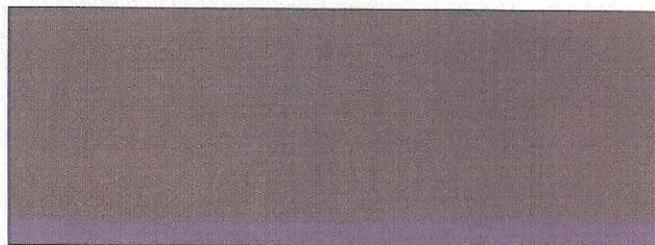


Dark Gray GY-210



White WH-835-S

### Image Refresh Backcourt / Under Canopy C Store Scheme/Trash Enclosures Monument Sign Bases / Solid Public Doors / Non Public Doors



Stone Gray 516-5

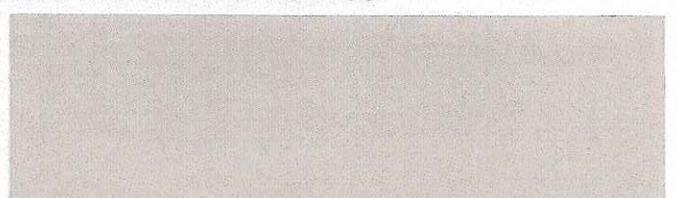


Southern Breeze 414-2

### HM21 Grand Entrance Backcourt Scheme (Legacy)



Havana Cream



Zephyr Hill

**Note: Turn Card Over For Additional Information**

PLAN ADDRESS: 15355 MAPLE VALLEY HWY  
RENTON, WA 98058

APPLICATION DATE: 06/20/2014

DESCRIPTION: The applicant is requesting approval of a fuel station (Cedar River Chevron) with a convenience store and car wash located in the Cedar River Station development at 15355 Maple Valley Highway (APN 232305-9211) on the south side of Maple Valley Highway (SR 169) and east of 152nd Avenue SE. The project site is a 0.8 acre (35,031 square feet) parcel and has a land use of Commercial Corridor (CC) and a zoning designation of Commercial Arterial (CA). The property is also located within Urban Design District 'D', and therefore subject to additional design elements.

In addition to a fuel station with underground storage tanks, the site will include a canopy with six double-sided pumps, a convenience store, and an automatic tunnel carwash. Primary access to the site is proposed off of Maple Valley Highway. The City of Renton's Critical Areas maps indicate the presence of seismic hazards related to potential liquefaction of soils during an earthquake event. There is an existing development agreement for the subject property. There are no trees on site. The proposal includes SEPA review, parking modification, and a vehicle stacking modification to enter the fueling area. The submittal included construction mitigation, geotechnical report, drainage report, and traffic study.

**Engineering Review**

Jan Illian Ph: 425-430-7216 email: jillian@rentonwa.gov

Recommendations: EXISTING CONDITIONS

- WATER Water service will be provided by Cedar River Water and Sewer District.
- SEWER Sewer service will be provided by Cedar River Water and Sewer District.
- STORM There are storm drainage improvements in Maple Valley Highway (SR169)
- STREETS There are no frontage improvements fronting the site on Maple Valley Highway (SR169).

CODE REQUIREMENTS

WATER

1. A water availability certificate will be required to be submitted to the City with the site plan application.

SANITARY SEWER

1. A sewer availability certificate will be required to be submitted to the City with the site plan application.

SURFACE WATER

1. A surface water system development fee of \$0.491 per square foot of new impervious surface will apply. This is payable prior to issuance of the utility construction permit.
2. A drainage report and drainage plan was submitted and approved under the original site plan for the Cedar River Station site. Installation of the onsite storm and detention/water quality vault is currently being constructed including a storm stub to the fuel station. Review of the onsite conveyance system for the fuel station site will be done at construction permit submittal.
3. A Construction Stormwater General Permit from Department of Ecology will be required if grading and clearing of the site exceeds one acre. A Stormwater Pollution Prevention Plan (SWPPP) is required for this site.

TRANSPORTATION/STREET

1. Frontage improvements along SR-169 are currently being installed by the developer of Cedar River Station.
2. A traffic impact analysis was reviewed under the Cedar River Station with the site plan application.
3. Unless amended or terminated, Section 4 of the Developer Extension Agreement for Aqua Barn Ranch LLC. (Recording # 20070307000134) acknowledges that the City agrees that a sum equal to \$252,799.50 be credited against the City of Renton's traffic mitigation fees.

GENERAL COMMENTS

1. Separate permits and fees for storm connection will be required.
2. All construction utility permits for drainage and street improvements will require separate plan submittals. All utility plans shall conform to the Renton Drafting Standards. A licensed Civil Engineer shall prepare the civil plans.

**Reviewer Comments**

Leslie Betlach Ph: 425-430-6619 email: LBetlach@rentonwa.gov

**Community Services Review Created On: 07/10/2014**

There are no impacts to parks.

**Planning Review Created On: 07/24/2014**

1. RMC section 4-4-030.C.2 limits haul hours between 8:30 am to 3:30 pm, Monday through Friday unless otherwise approved by the Development Services Division. The Development Services Division reserves the right to rescind the approved extended haul hours at any time if complaints are received.
2. Within thirty (30) days of completion of grading work, the applicant shall hydroseed or plant an appropriate ground cover over any portion of the site that is graded or cleared of vegetation and where no further construction work will occur within ninety (90) days. Alternative measures such as mulch, sodding, or plastic covering as specified in the current King County Surface Water Management Design Manual in the dates of November 1st and March 31st of each year. The Developer shall complete the permit. prior to final inspection and approval of

3. Commercial activities shall be restricted to the hours between seven o'clock (7:00) a.m. and eight o'clock (8:00) p.m., Monday through Friday. Work on Saturdays shall be restricted to the hours between nine o'clock (9:00) a.m. and eight o'clock (8:00) p.m. No work shall be permitted on Sundays.
4. All landscaping shall be irrigated by an approved irrigation system prior to final occupancy permits.

**Fire Review - Building**

Corey Thomas Ph: 425-430-7024 email: cthomas@rentonwa.gov

Recommendations: Environmental Impact Comments:

1. The fire impact fees are applicable at the rate of \$0.54 per square foot of building area. No fees charged for the pump island canopy. These fees are paid at time of building permit issuance.

Code Related Comments:

1. The preliminary fire flow requirement is 2,500 gpm. Three fire hydrants are required. One fire hydrant is required within 150-feet of the proposed buildings and two hydrants are required within 300-feet. Existing hydrants can be counted toward the requirement as long as they meet current code including 5-inch storz fittings. A water availability certificate is required from Cedar River Water and Sewer District.

2. An approved fire alarm system is required throughout the building. Separate plans and permits are required to be submitted to the Renton Fire Department for review and permitting. Fire alarm system shall be fully addressable and full detection is required.

3. Separate plans and permits are required to be submitted to the Renton Fire Department for review and permitting for all underground flammable liquid storage tanks and tank monitoring systems.

4. Fire department apparatus access roadways are required to be minimum 20-foot wide fully paved, with 25-foot inside and 45-foot outside turning radius. Fire access roadways shall be constructed to support a 30-ton vehicle with 322-psi point loading. Access is required within 150-feet of all points on the buildings.

5. An electronic site plan is required to be submitted to the Renton Fire Department for pre-fire planning purposes prior to occupancy of the building.

6. City ordinance requires full back up on site generator capacity to keep the station up and running during long term power outages.

**Police Review**

Cyndie Parks Ph: 425-430-7521 email: cparks@rentonwa.gov

Recommendations: Minimal impact on police services.